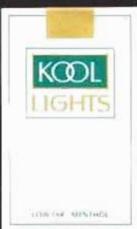
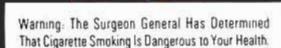
Woodworker's Shop Fopular Vechan 800,000-MILE REPORT **FORD MUSTANG: Owners Praise** Sporty Styling And Handling PM's OUTDOOR ADIRONDACK FURNITURE CAR STEREO: Add-Ons That Make A Good Set Great! PLANS: BUILD OUR GIANT GARDEN CART HOW COMPUTERS
ARE DESIGNING YOUR
NEXT CAR **Battlewagon New Jersey Back At Sea With Rocket Punch**

KOOL LIGHTS

There's only one way to play it.

There's only one sensation this refreshing. Low 'tar' Kool Lights. The taste doesn't miss a beat. Kings and 100's.





Kings, 9 mg, "tar", 0.8 mg, nicotine, 100's, 10 mg, "tar", 0.9 mg, nicotine av, per cigarette by FTC method

C.1962-B&W 7.5ni



Few things in this world are built like Ruth Gordon and the new Subaru Brat.

> WHAT OTHER BODIES COMBINE SO **MANY IMPRESSIVE FEATURES?**

Like Ruth, the Brat is tough. And it has a personality all its own. Besides all new styling, the Brat offers outdoor bucket seats and a Halo Twin Roof."

GREAT PERFORMERS.

The Brat delivers the best gas mileage of any four wheel drive

vehicle. (27) EPA est. mpg, 36 est. highway mpg.** What's more, the Brat has Front and On Demand Four Wheel Drive.

> **ENGINEERED LIKE FEW THINGS** ARE ENGINEERED.

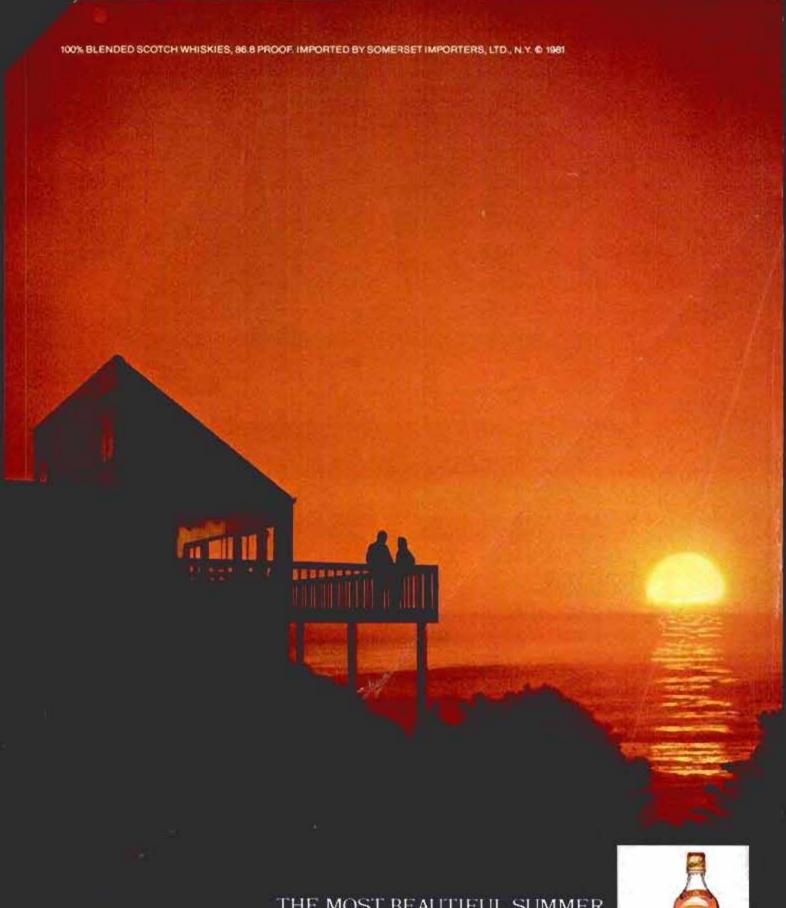
Most importantly, Ruth and the Brat are built to last. So if you're the kind of person who appreciates quality
workmanship, look into Ruth Gordon and
the new Subaru Brat.
Each sold
separately.



THE NEW SUBARU BRAT.

'AVAILABLE ON GL MODELS.

** 1982 EPA ESTIMATES. USE EST, MPG FOR COMPARISONS. YOUR MILEAGE MAY DIFFER DEPENDING ON DRIVING SPEED, WEATHER CONDITIONS AND TRIP LENGTH. ACTUAL HWY, MILEAGE WILL PROBABLY BE LESS. SUBARU OF AMERICA, INC. 1982



THE MOST BEAUTIFUL SUMMER EVENINGS START WITH RED.

JOHNNIE WALKER* RED THE RIGHT SCOTCH WHEN ALL IS SAID AND DONE





On the cover

When it puts to sea for trials in two months, the USS New Jersey will be the world's only active battleship Our artist's view shows the awesome offensive power of the ship provided by its mix of neavy guns and modern missile systems. The story on page 73 tells how the Navy sees the role of its 'new" capital ships -PM painting by Ed Valigursky



JUNE 1982

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What do you do when the load's too heavy for your little Chevy?



Take the Coleman Caboose.



The new Coleman Caboose lets your quick little car carry like a wagon.

The Caboose is light, lithe and rolls like a royce. Just the ticket for spruces and spring seedlings, bricks and boards, luggage and leisure gear, or the antiques you can't pass up at the auction.

You can even get an optional top that keeps out weather, and locks to keep out intruders.

And when your haul is over, the Caboose stands on end and hides against a garage wall, fence or utility shed.



The Coleman Caboose. It's fun. It's functional. And it only costs about as much as a good set of radials.

Hubidaps available at accessory

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HANDYMEN, MEET YOUR MAKERS.

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7%" Circular Saw:
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ejection chute,
and permanent
lubrication.

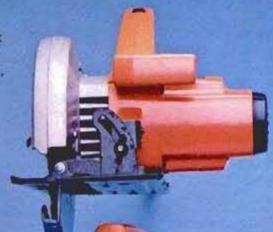
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Black & Decker Vs-HP Variable Speed Jigsaw: Featuring fingertip speed control, callbrated tilting shoe, detachable cord, and no-ground double insulation.

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Black & Decker Value Plus power tools are on sale from May 31 through June 19 at over 1,900 K mart stores across the U.S.A.

Black & Decker quality at a K mart price. Nice.



LETTERSTO THE EDITOR

Some like 'em hot

As a woman considering a new highperformance car, I enjoyed Joe Oldham's article Detroit Puts Muscle Back In (page 109, Mar. '82). But in his review of the Mercury Capri, he says: "Most women won't like this car. Most men will."

Unless Oldham has had a sexchange operation, he doesn't know what women will like. I, and many of my women friends, enjoy a car that really goes.

> KRISTI CARLSON LISBON, N.D.

What happened to the Chevrolet Citation X11 and Pontiac Phoenix SJ? I was under the impression these cars were made in Detroit.

ALVIN WASSERMAN MIAMI BEACH

Oldham writes in glowing terms about superfast acceleration, smoking tires and beating out the next guy at a stoplight. People with driving habits like these are bad news for everyone on the road. A car is a piece of heavy machinery that can mangle people and other cars when driven without caution.

Our roads are crowded with cars, pedestrians, cyclists and children. No one can drive in a vacuum.

> FRED DEVIN NEW HAVEN, CONN.

Author Oldham replies: Maybe our roads wouldn't be so crowded if people didn't poke along at ridiculously low speeds. Seriously, though, we were talking about what seems to be a peculiarly American phenomenon; we certainly weren't advocating reckless driving habits.

We reviewed the Chevy Citation X11 in our January '82 issue (An American Sport Coupe Takes On Four Imports, page 88). It blew the doors off the foreign competition. Pontiac wasn't advertising its Phoenix SJ as a performance car until recently. We tried to get a Trans Am for our muscle-car test, but only one had been built at the time and it was not available.

Missing windmill mystery

Motorists on Interstate 10 near Palm Springs, Calif., will not see those two giant windmills you showed in your March '82 issue (PM Briefs, page 96). The vertical Darrieus rotor blew to pieces the first

time it was started, and the propeller type had to be dismantled shortly thereafter because of problems.

If NASA had acted with the same efficiency on the space program, John Glenn would still be waiting on the launch pad.

ADAM BALAS PALM SPRINGS, CALIF.

It's a bit embarrassing, we admit, but they sure were there when the photo was taken. Word from DOE is that the Darrieus rotor has been temporarily abandoned, but the propeller type is being rebuilt and will be back in service soon.

PM's treetop hideaway

Here's a photo (below) of the tree house I built from your plans (Build This Treetop Hideaway For Your Youngsters, page 92, Aug. '80). Because my trees were farther apart



PM's tree house, as built by reader Mario Albano. Open porch ahead of roofed area is nice added feature where space permits.

than the ones you showed, I used 2×8s for the main support beams instead of 2×6s. I also had room to add an open porch in front of the roofed section.

The children love the house, but there's one problem: Now they want me to build furniture for it.

> MARIO ALBANO SHREVEPORT, LA.

Sorry we got you into more work, but you did a great job on the structure. For others who may be interested in this project, photocopies of the article are available for 75 cents from Popular Mechanics, Box 1014, Radio City Station, New York, N.Y. 10101.

No hot air

It was great to see your article Now We're Ballooning On Solar Power (page 100, Apr. '82). In 1967, I built a

model of Lee Ashman's solar-powered balloon for a science fair project. My teacher said I was wasting my time and should be working on something "important."

Funny how times change.

SCOTT PARKER BEAUMONT, TEX.

Zap-you're alive!

I'm very pleased with the NiCd battery rejuvenator I built from your plans (Zap Your NiCd Batteries To Life, page 62, Feb. '82). It has made it possible for me to rejuvenate NiCds that I was unable to restore by any other means.

> CHARLES J. ASEA PARSIPPANY, N.J.

Automated gate

In a recent Homeowners' Clinic (page 32, Jan. '82), a reader inquired about using an electric garage-door opener to operate a fence gate. We did exactly this to control a 12-foot sliding driveway gate. It works great.

I used a standard opener, modified to operate at 90° from its intended position. I lengthened the rail, chain and cable, readjusted the stop switches to accommodate the longer travel and enclosed the motor assembly in a waterproof housing. Total cost: \$198.

Anyone wishing more information about this procedure is welcome to write to us.

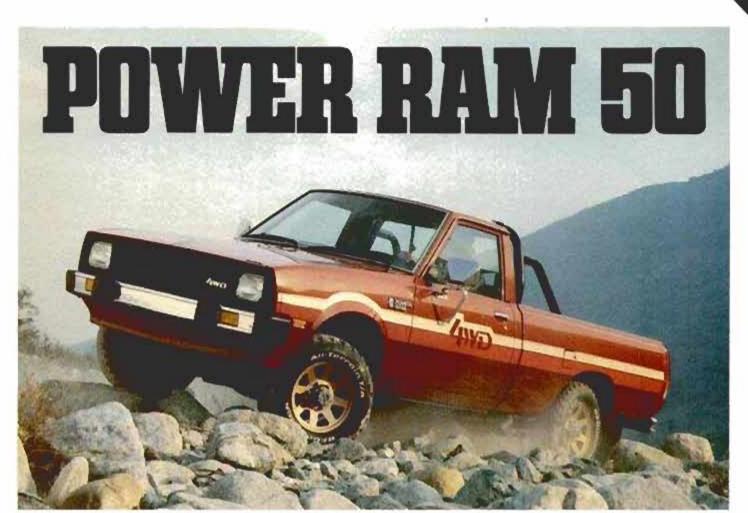
GUS FOSSUM 42052 3rd ST. E. LANCASTER, CALIF. 93535

A cool idea

In your March '82 issue (The PM Garage, page 74), you say that electric cooling fans in cars are more efficient than belt-driven ones. How can it be more efficient to run a fan off the generator, which is driven by the engine, than directly off the engine itself?

W. B. DELONG HOCKESSIN, DEL.

Auto editor Tony Assenza replies: An engine-driven fan uses 3 to 5 hp and runs all the time, even when it doesn't have to, such as during highway cruising. But a thermostatically controlled electric fan comes on only when water temperature indicates it's needed, and it runs on reserve power already available. Thus, the saving.



Dodge introduces the "Four Wheeler of the Year."

"...the Power Ram 50 is one of the all-around great mini 4x4 pickups to hit the market." Four Wheeler, Jan. 1982

"...any way you slice it, the Power Ram 50 is the best there is among small 4x4 pickups." Pickup, Van & 4WD, Jan. 1982

"This is not a mini pickup like all the others from Japan but rather a mini 4x4 that rivals the power and performance of any (small) pickup you choose to stick it against."

4 Wheel & Off-Road, Jan. 1982

That's a sampling of what the experts have to say about the new Dodge Power Ram 50. Here are some key facts behind the raves.

More hauling power than Datsun and Toyota.

Power Ram 50 hauls the biggest standard payload in its wheelbase class. It's got the biggest engine you can get in a small 4x4 pickup. Most horsepower, most torque. Its independent front suspension shows you true grit over rocky terrain.

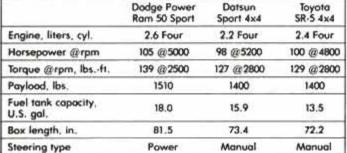
Drive it yourself. Odds are you'll discover what the experts

Dodge Power Ram 50 Sport Sport 4x4 SR-5 4x4 Square Ram 50 Sport 2.2 Four 2.4 Four Rosepower @rpm 105 @5000 98 @5200 100 @4800 Winners: In 1981 Dodge Ramcharger won Four Wheeler magazine's "Four Wheeler of the Year" award. Now Dodge Power Ram 50 is 1982's winner, and Off Rood magazine's "4x4 of the Year".



already know: in the tough world of 4-wheel drive, Dodge

Power Ram 50 is the toughest small pickup of them all.



^{*}Base sticker price excluding title, laxes and destination charges. Price as of 3-15-82



SCIENCE WORLDWIDE

Water, water everywhere

Calling all scientists: Be on the lookout for 260 million tons of water vapor missing each year. Originally, scientists thought the water that evaporates from the Earth's surface trickles back down as precipitation.

But Lawrence Livermore physics lab sleuth Hugh Ellsaesser has measured the vapor content of the stratosphere and discovered hundreds of millions of tons less vapor than expected. In a recent paper, NASA researcher Ed Danielson suggested that the missing vapor may get supercooled at high altitudes. float partway down, get warmed and rise again. This moving around keeps it from being detected. However, Ellsaesser notes, the large thunderstorm systems in which this supposedly takes place aren't massive enough to hide the vapor. Thus, the mystery continues.

Sunlight

Solar collectors of the future may look like arrays of fluorescent bulbs on a large rectangular frame. Researchers at the General Electric labs in Schenectady, N.Y., report that experimental collectors, using clear plastic glass tubes resembling fluorescent bulbs, gathered more sunlight than standard glass or plastic bubble covers of conventional collectors.

As the sun moves across the sky, the tube shape presents a greater surface area on which to shine. The glass-tube array sits atop slabs of high-temperature plastic foam that serves as a perfect insulator.

When the collector comes to market later this decade, GE expects it to gather three times more solar heat during a northern U.S. winter than conventional models.

Skin-chilling news

Skin taken from a dead donor's back, chest and thighs once could remain "viable" for only 24 hours. But Dr. Randolph May at St. Agnes Medical Center in Philadelphia now reports keeping skin viable for weeks by slowly freezing it to -385° F. for storage, then warming it to room temperature in 15 seconds when a transplant is required.

Viability is the skin's ability to retain moisture, control body temperature and prevent infection. After a few weeks, the body rejects the donated skin. But by then, a fresh layer of skin has grown back to do Donut-shaped magnets to hold fusion clouds



First of 22 big magnets is delivered to Princeton for the high-energy fusion reactor.

The "conservation" laws of physics say, in effect, there's no such thing as a free lunch. But a bargain is permissible.

Thus, Princeton's donut-shaped Tokamak fusion reactor will begin operations later this year. At temperatures of hundreds of millions of degrees Fahrenheit, the reactor will join single atoms of lightweight deuterium to form heavier elements, while also releasing vast amounts of energy.

Obviously, it takes energy to make energy. But the bargain comes from the fact that the plasma cloud produced in the reactor will yield more energy than required to create it—if you can keep the cloud from drifting apart.

That's why the Tokamak is lined with 22 high-powered magnets. The 11-ton coils use 1,500 feet of copper to conduct electricity. When wound, each has an innercircle diameter of 10 feet and a 14-foot outside diameter—a very tight fit. Westinghouse Electric delivered the first of the magnets earlier this year and will complete delivery by early winter. Princeton officials hope to generate a reaction that pays for itself by 1985.

the job. With the new technique, skin has been kept up to 80 percent viable for "several weeks."

New chemical group

A Purdue University team has discovered the first new class of "chondrite" meteorite in 150 years. (Chrondrites are the remains of meteors created at the beginning of the solar system.)

Examining a meteorite that fell near Cumberland Falls, Ky., chemist Michael Lipschutz and his assistants dubbed the group "fosterite," after the iron-magnesium fosterite mineral particles found inside the meteor. The chondrite drew the chemists' attention because of its odd interior color—ash white with jet-black speckles. Lipschutz says that analysis has shown it contains carbon, which is odd for a meteorite.

Whistle stop

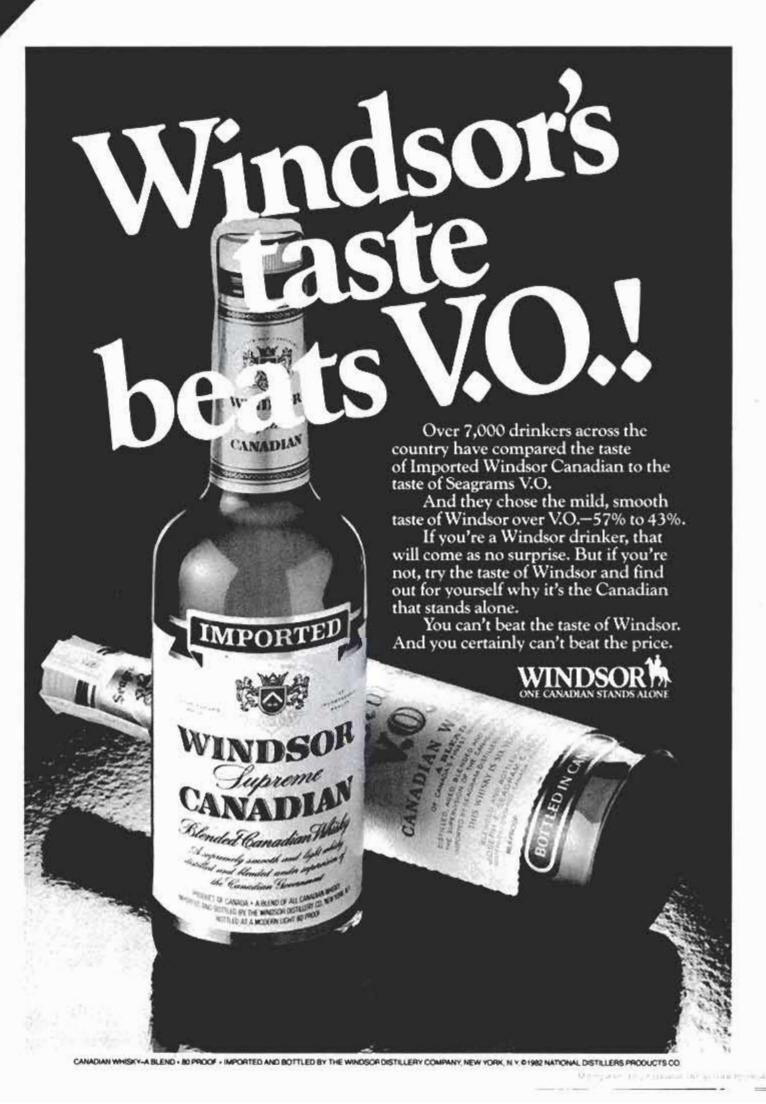
An unidentified federal agency was about to grant money to a scientist whose work had been exposed as a fraud, when "someone blew the whistle," according to Rep. Albert Gore Jr. (D.-Tenn.). But members of the congressional science and technology oversight subcommittee—chaired by Gore—met with officials of the agency and the grant money was quickly withdrawn.

In an exclusive interview with PM, Gore declined to name the agency, but said the subcommittee probably will hold hearings later this year, because he and other members aren't "entirely satisfied with the response" of federal agencies to charges of unchecked fraud by scientists working under federal grants.

During hearings last year, the House panel uncovered several incidents in which scientists knowingly falsified data to get papers published or to qualify for grants.







IMPORTS&MOTORSPORTS



Spring was in the air when the Geneva Motor Show opened in March, so I made a beeline for the new convertibles

Best of the bunch, hands down, and top down, is the Porsche 911SC Cabriolet. It's the first all-open car from Porsche since the 1965 Model 356, and it's not to be confused with Targa models which have the builtin roll bar. The cleverly designed soft top lets the Cabrio retain 2+2 seating, and the top's aerodynamic profile, similar to the Coupe's roof, lets the Cabrio run just as fast: 145 mph top speed. There's no production convertible that's faster. One of the options with the Cabrio, which arrives here later this year with a sticker topping \$30,000, is a good ol' tonneau cover. You merely unzip the driver's side and buckle in to blast off.

Not as fast, but maybe even more fun is the much more affordable ragtop Samba. This Everyman's convertible, from the French Talbot firm, is a Pininfarina design that I | tures. What a neat roadster!

would love to see marketed here. No such plans are in the works, however, so VW, with its Rabbit convert, continues to have the four-seat, open-air market to itself.

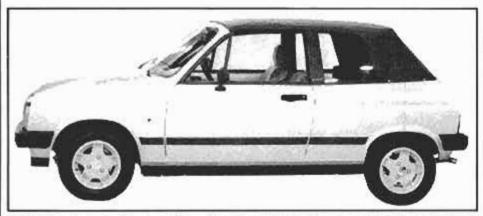
Show stopper among the nonproduction design exercises was from



Corsa roadster is a show car from Opel.

none other than General Motors. The Opel Corsa show car is a fourseater that's based on the European S-car that will be built in Spain and debut later this year. The production car will be nothing like this one, though.

There's a fairing behind the driver's seat, making the car look like a little racer, and other unique fea-



Talbot Samba is a mini-convertible with economical 1,360-cc Four and lots of charisma.

Turbos galore

Making its world premiere at Geneva was Mitsubishi's Starion, a turbocharged 2+2 fastback that'll be the eve-catching car in the new Mitsubishi dealer network. It'll have to buck the tough RX7/Supra/Z-car triumvirate. In Europe, all of Mitsubishi's cars-Starion, Sapporo, Colt, Gallant and Lancer-are offered with turbos

Turbos for diesel and gas engines are showing up everywhere: Rolls-Royce previewed the Bentley Mulsanne Turbo, one hot luxury sedan,



Mitsubishi Starion Turbo arrives in '83.

with the turbo boosting top end to 130 mph and cutting 0 to 60 to 7.4 seconds. Then there are the VW-Audi cars, which are getting turbodiesels almost across the board. We'll be seeing some this fall.

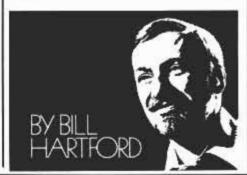
Gimme a break

A new version of Peugeot's 505 made its debut at the Salon International de l'Automobile: It's the wagon, of course-or "break" as they call it in



Peugeot 505 wagon will get a turbodiesel.

Europe—and it will delight Peugeot fans who have tired of the bulbous old 504 wagon. So, if you're all ready for a really elegant hauler, this is the one.





that'll make you glad Power Painter.

Every once in a while you come across a product that offers significant advantages over a tried-andtrue method of doing things.

In the case of painting, consider a Wagner. Power Painter. instead of a brush.

A Wagner Power Painter's speed should quickly make a believer out of you. Just among the examples we've shown, the time saved is astonishing.

And a Wagner Power Painter is ideal for dozens of other painting chores around the house.

Professional-looking results made easier

Of course, speed isn't a Wagner Power Painters only long suit.

A Wagner Power Painter also makes it remarkably easy to achieve professional-looking results-the spray lays down a smooth, even finish with no brush marks.

And when it comes to intricate painting jobs like wrought iron railings, louvered doors and bicycles, it's amazing the way a Wagner Power Painter reaches into crevices and applies paint.

That's also why a Wagner Power Painter is so effective on rough, textured surfaces like shake shingles.

Spray paints, lacquers. stains, varnishes, and more

Virtually any kind of latex or oil-based paint is compatible with a Wagner Power Painter.

It can also apply stains, varnishes and lacquers. Or spray almost any other liquid, even pesticides.

So a Wagner Power Painter is a lot more than just a "one project" tool.

And cleanup is simple-just run the appropriate solvent through until its clean.

No air compressor

The secret to a Wagner Power Painter is a precision electric, pistondriven pump.

It creates a pressure that propels liquid out through the tip, which

then forms the spray.

There's no bulky air compressor to lug around. And significantly reduced fogging that usually accompanies painting with compressorpowered sprayers because a Wagner Power Painter doesn't mix the paint with air.

Helpful attachments

A variety of helpful attachments make Wagner Power Painters even more versatile.

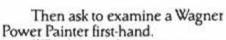
Complete outlits include things like a flexible spray tip designed exclusively for floors and ceilings.

And a tube that replaces the reservoir, so you can draw paint right from the can.

You can even get an optional spray tip that narrows the spray for precision painting.

But rather than simply reading about a Wagner Power Painter, we suggest you visit your local hardware store, paint store, home center or wherever quality paints are sold.

1. 1982 Wagner Spray leah Corp.



Whether you're painting your '57 Chevy or the broad side of a

barn. you'll discover there's a Wagner Power Painter that's just right for you.

And we think you'll also discover it's high time you owned one.

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NOTES FROM THE

t was one of those small. but telling, points," said PM's Steve Willson to sum up a visit to Ernest Glaser's shop (page 94).

While examining some pine cabinets Glaser made. Steve asked why the drawers worked so well. Glaser revealed that he had carefully laminated maple strips to the drawer bottoms and cabinets to gain such effortless movement-instead of using commercial slides.

It was a simple technique. and nothing to rival the elegance of his furniture makthe work of a craftsman.



ing. But, such details mark The cutting edge: PM's Willson makes time-honored inspection in a fine craftsman's great shop.



Out of the Fuego and into the bunkers: Auto buff Assenza becomes a war-history buff while at Omaha Beach.

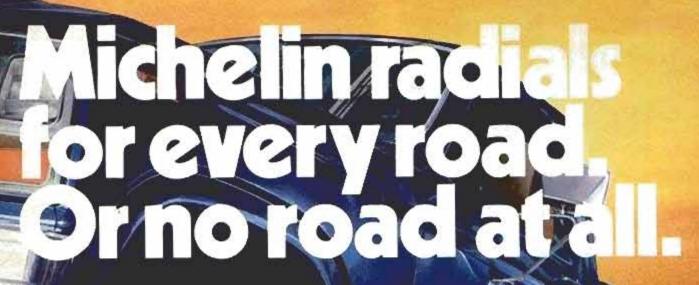
Are car buffs truly fighter pilots at heart? Seems so, PM's Tony Assenza, after testing a new Fuego and Renault 9 (page 90), toured Normandy's war sites.

Devoted as he is to cars, Tony was finally moved to murmur: "I'd even turn in my driver's license to have been in the cockpit of a P-51 here."



Time capsule: Hand-set type still remains in vated Iowa is not going to have the mothballed lowa's newspaper shop, the same flavor.'

Touring the mothballed lowa for our born-again battleship story (page 73), Technology Editor Robin Nelson reports he felt he had entered a time capsule. He found such relics as a complete. 1940s-style soda fountain. But most interesting was the ship's newspaper print shop-with drawers of hand-set type. "Somehow," says our bemused technologist, "I think news that crawls across the inevitable TV monitors of the reacti-





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a major new porcelain plate collection ...

WATER BIRDS OF THE WORLD

by Basil Ede

An exciting new series of twelve collector's plates by Europe's foremost wild bird portraitist.

Each plate bearing an original work of art created by Basil Ede exclusively for this collection.

Each decorated with a border of pure 24 karat gold.

Available by subscription only. Advance subscription deadline: July 31, 1982

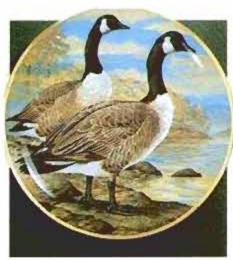
THE ARTIST Basil Ede, of Sussex, England, has been called "the outstanding wild bird portraitist of his generation, and perhaps of his cen-

tury." He is widely regarded as Europe's most distinguished bird artist. Indeed, the authority of his style and the distinction of his work have profoundly influenced the way birds are portrayed by artists throughout the world.

After notable exhibitions in London and other major European cities, Basil Ede was honored by a one-man show at the Smithsonian Institution's National Collection of Fine Arts in Washington, D.C. This was followed, during recent years, by important exhibitions at New York's famous Kennedy Galleries.

His paintings have been commissioned by the National Audubon Society and the World Wildlife Fund, among others. And he is represented in many public and private collections, including the Smithsonian Institution and the collection of HRH Prince Philip, Duke of Edinburgh.

Now, at the height of his career, Basil Ede has created an important new series of superlative works of art in fine porcelain - Water Birds of the World. Twelve



CANADA GOOSE

collector's plates portraying the wild beauty and majesty of water birds in precise, authentic detail - with Ede's inimitable flair for color and composition.

Each of these twelve plates is, in itself, a masterful work of art. Together, they form an incomparable collection that will be a proud acquisition for every sub-



WOOD DUCK

scriber ... a focus for conversation and admiration when displayed in the home.

THE WATER BIRDS PLATES. The plates will be large in size - 9 inches in diam-eter - to provide full scope for Ede's finely detailed portrayals of water birds in their natural habitat.

And they will be crafted under the supervision of Franklin Porcelain in Japan, by craftsmen schooled in the delicate, demanding art of fine porcelain.

Franklin Porcelain has devoted more than two years to meticulous preparation for the issuance of these plates, and every detail will be of the highest quality. For example, each plate will be handdecorated with a border of pure 24 karat gold. And each plate will incorporate as many as sixteen separate ceramic colors.

Ede's very beautiful and marvelously accurate works of art-created especially for this collection and available only on these fine porcelain platesmake this an acquisition that will be enjoyed and prized by all who love the beauty of birds, of art, and of porcelain.

THE SIGNATURE EDITION. The first edition of this collection will be a special Signature Edition. Each collector's plate in this limited edition will bear Basil Ede's signature on the back in 24 karat gold. This signature will appear only on the plates in this first edition.

The plates in this unique edition will be crafted exclusively for individual subscribers. The edition is available only until the end of 1982. And the limit of one collection per subscriber will be enforced without exception. Thus, the total number of sets of the Signature Edition will be forever limited to the exact number of original subscriptions entered during the offering period.

ADVANCE SUBSCRIPTION DEADLINE: July 31, 1982. The Signature Edition will be issued to subscribers at the conven-

ient rate of one plate every other month. The original issue price of just \$55 per plate is payable in two convenient monthly installments of \$27.50 per month. Each plate will be accompanied by specially written reference material and a special wall mount. And a Certificate of Authenticity will accompany each collection.

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PM ELECTRONICS MONITOR

Portable videotext

I stood in the plush, combination lobby and casino of Las Vegas's famed Caesars Palace hotel. Around me swirled the glittering girls and jaded, pinky-ringed men who throng to that city—to win or lose. But before me stood one incredulous hotel clerk who could only, in massive befuddlement, repeat my own words to me.

"You want to change your room to one with a modular phone plug?" he asked me. His voice hinted at his suspicion that I had discovered a

new perversion.

"Well," I explained, "I have this new computer... not really a computer—more a terminal. It's the new RCA VP3501... and I'd like to plug it in and...." The clerk's eyes were glassy. He motioned for the night manager to join our conversation. Joe Oldham, PM's Executive Editor who was attending an electronics show with me, mumbled an excuse and hurriedly detached himself from my side to vanish among the slot machines and showgirls.

On my own, I explained, as best I could, to the Caesars nighttime staff:

"This terminal looks like a flat typewriter keyboard; you just plug it into any phone line and attach it to a TV. Then you can call any of the national computer services."

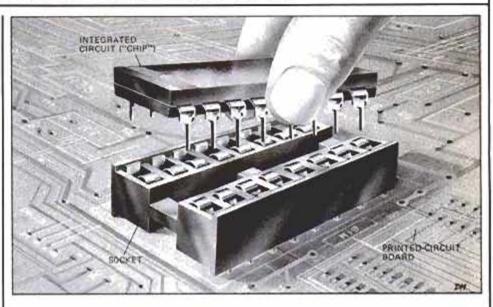
"Computers?" the night manager looked at me suspiciously, her eyes heavily lidded. "Why would you need to call a computer?" Off to the side, a million bells and sirens announced suddenly that at least one jackpot had been won, temporarily drowning out the moans and curses of the losers.

I wondered if I should tell her about MicroNET and other such computer networks—that the terminal would allow me to stay in touch with friends throughout the country via electronic mail and conference-style typing. Or perhaps I could mention the up-to-the-minute news and features. Then again, I could explain how I could have access to various programming languages to while away my after-losing hours.

But I had hesitated too long.

"Surely," she said, "you realize that finding one room with a modular phone plug out of all our rooms would be," she paused, "like looking for a needle in a haystack." A slight smile at her own originality flickered at the corners of her mouth.

"Oh, well, thanks," I mumbled and



Socket to 'em

Have you ever finished an electronic project which uses integrated circuits, turned it on and watched it just sit there? If so, you may know how difficult, annoying and often impossible it is to desolder "chips" without hurting the print-

ed-circuit or perfboard. The solution is simple: Use sockets—all the time. DIP (dual in-line packaging) sockets sell for under a dollar, but they can be worth a million when you really need them.

turned away. My heart fell. I was alone in Caesars Palace, Las Vegas, without even a computer terminal. I would have to make do.

Later in the week, in the far less boisterous realm of Detroit, I again checked into a hotel without the right style phone jack. But at least I would be able to play with the automakers' computers in Motown for a PM story (see How Computers Bring Cars To Life, page 76).

A real test of the VP3501 would have to wait until I got home.

Plugging in

Back on familiar turf again, I connected the terminal to a modular phone jack—the kind that's available in most private homes. Installation took only about five minutes. Then, at last, I was able to put this exciting new machine through its video paces.

As I had told the hotel people, the VP3501 looks like a flat, compact keyboard. Its keys are membrane switches (if you've seen the Atari 400 computer or Odyssey² home-video game, you'll note that they have similar keyboards).

Dialing MicroNET (the terminal comes with an application to join that network; costs are \$5 per hour), and connecting took only a minute or two.

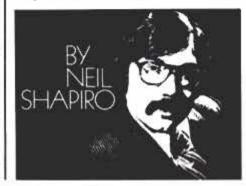
The VP3501 lets the user choose from six bright colors (or black) as a background on the TV screen and also change either the intensity or color of the scrolling type.

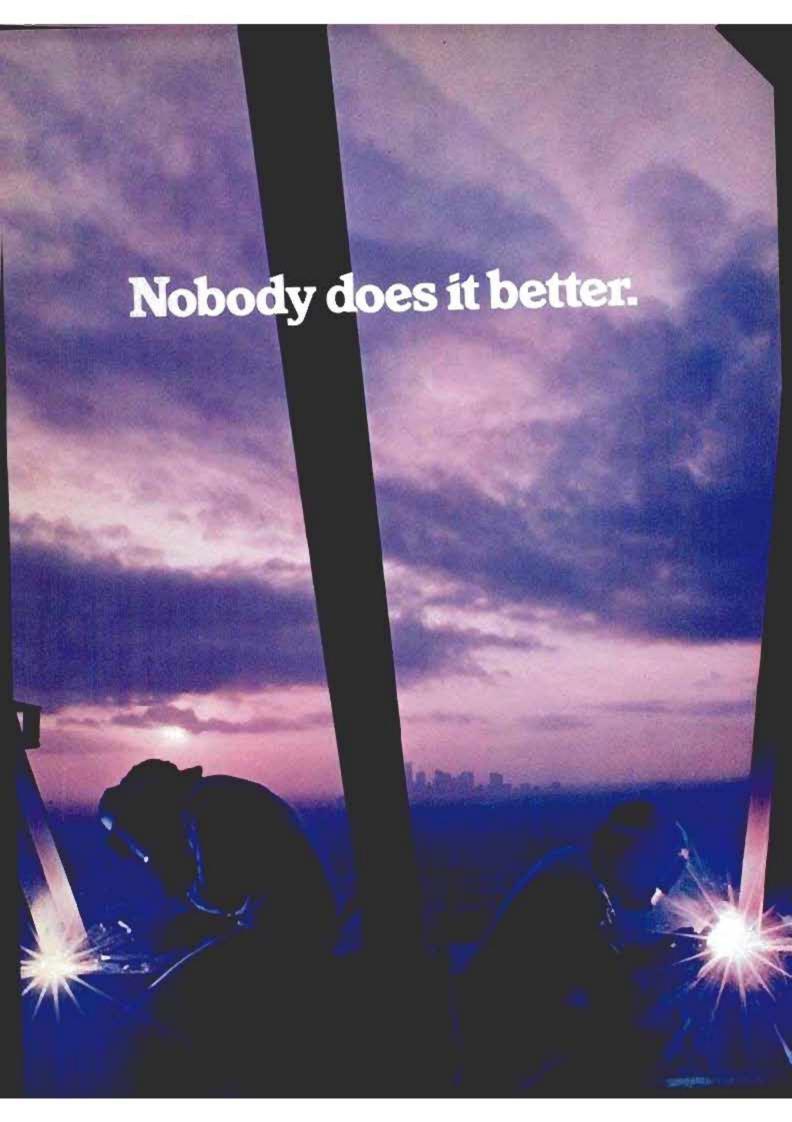
Even the membrane keyboard was easy to get used to. Luckily, the RCA engineers had thought to have the TV speaker "beep" whenever a key was typed, so the absence of a typewriter "feel" didn't matter.

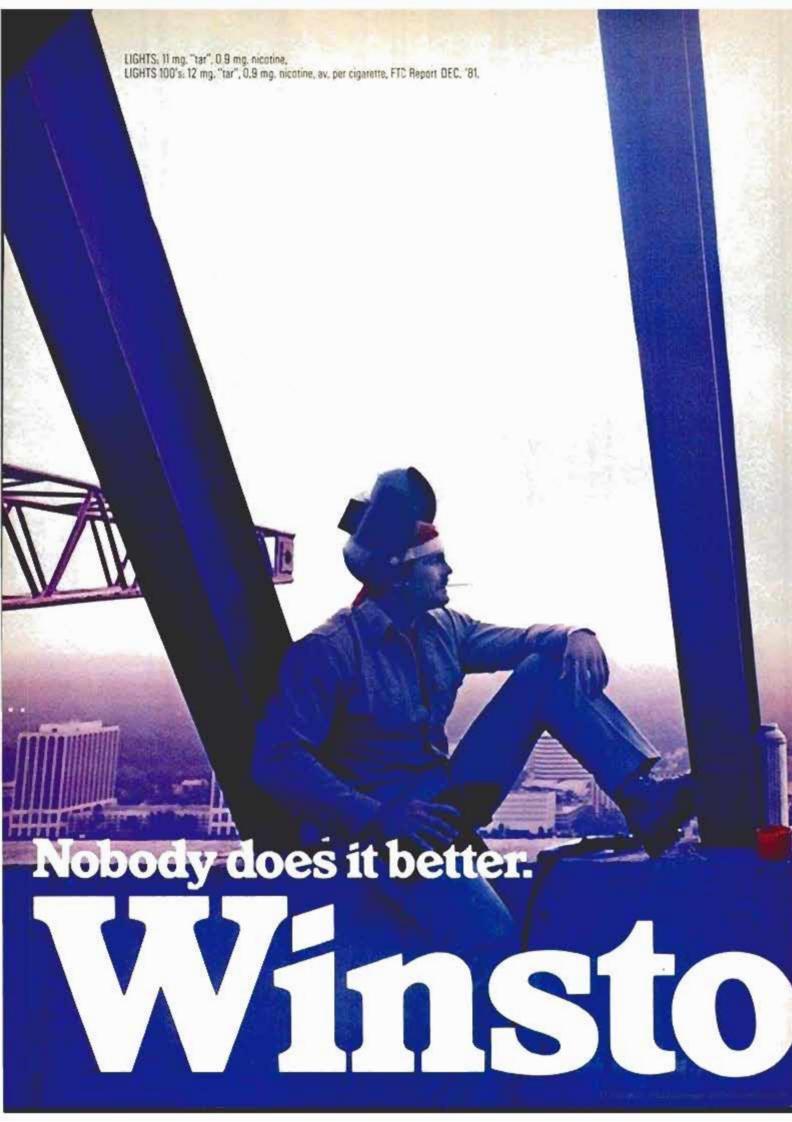
At about \$400, this new terminal is a first in inexpensive at-home or office telecommunications. It was simple to use (complete with an excellent instruction manual) and reliable.

If all you need is communications and not a computer, this is a good buy. Or if you want to add a portable terminal to an existing computer or network, this is the most portable one I've seen that offers a full keyboard and a 40-character by 24-line display.

Last I heard, RCA was even coming out with an attachment that would let you use any telephone maybe even Caesars'.







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HOMEOWNERS' CLINK

Soundproofing a room

My bedroom is on the second floor of a remodeled house. The studs are 2×6s with 6 in. of insulation in the walls. I sometimes work night shifts and sleep during the day. Since my neighborhood is quite noisy, I wondered if you could tell me how to soundproof the room inexpensively?—Jeff Nelson, Virginia, Minn.

ly?—Jeff Nelson, Virginia, Minn.

The National Bureau of Standards book, NBS Building Science Series 104, quotes from the 1974 ASHRAE (American Society of Heating, Refrigerating and Air-conditioning Engineers) Handbook of Fundamentals that "noise within a room is absorbed by draperies, rather than reflected as from uncovered glass. Also, outside noise transmitted through the glass is partially absorbed (by draperies). The denser the weave and the heavier the drapery, the more effective it is in reducing noise transmission." The same

book mentions that storm windows and weatherstripping aid in stopping sound.

I would add storm windows and, if space permits, a floor-to-ceiling drape the length of the outside wall. A thermal drape installed on a traverse rod would also help.

Insulation removal

The local roofing company that just reshingled my roof told me to remove the fiberglass insulation installed between my ceiling rafters. I installed this insulation about 10 years ago with the vapor barrier facing the living space. When the new roof was put on, I also had a thermostatically controlled fan installed on my roof. The roofer said that unless I removed the insulation, the shingles would curl. Is he right!—James Barber, New Castle, Pa.

James, unless you're not telling me all, I think the roofer has a part ownership in your heating-fuel supplier. We can't imagine why he would have given you such advice.

Blazing campfires

Our fireplace is about 4 feet below grade in our trilevel house. Two or three days after we have a fire, the family room still smells like a campfire. I added a draft stopper on top of the chimney, installed glass doors and have had the chimney cleaned professionally. However, the problem persists. Any suggestions!—Craig G. Letchner, Livonia, Mich.

Craig, I've checked several authorities, but can only give you some general theories here. The odor could be the result of flow reversal, a situation that is most typical of basement fireplaces in multistory houses. When the temperature of the air within your chimney is lower than in the house, the air in the chimney can flow downward. The higher the chimney, the colder the air and, thus, the better chance you have of downdrafts. Also, your house can become slightly depressurized from exhaust fans in the kitchen, bathroom and clothes dryer. This also causes flow reversal.

To prevent the latter from happening, introduce outside air into your home with a vent to your fire-place, for example. This is discussed in "Having And Eating Cake" (Homeowners' Clinic, page 70, June '80). Such problems are covered thoroughly by Jay Shelton in his excellent book, Wood Heat Safety; it's \$10.95 postpaid from Garden Way Publishing, 1508 Ferry Rd., Charlotte, Vt. 05445.



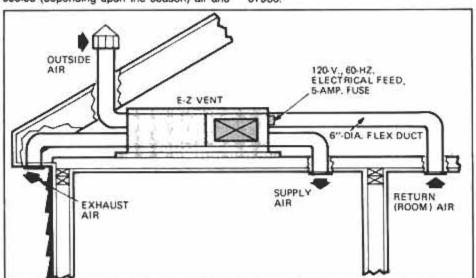
Do you have a home maintenance or repair problem? Send it to Homeowners' Clinic, Popular Mechanics, 224 West 57th St., New York, N.Y. 10019. Letters cannot be enswered individually, but problems of wide interest will be discussed in this column. For more home repair and maintenance help, get PM's Home Care Guide, \$4.95 postpaid. Send orders(s) to PM, Box 1014, Radio City Station, New York, N.Y. 10101

SERVICE TIP

In past Homeowners' Clinics, I've mentioned the need—under certain conditions—to introduce fresh, outside air into the living environment. The main problem to overcome is the prewarming of the air in the winter, and the precooling of it in the summer prior to distributing it in the house. E-Z-Vent, by Des Champs Laboratories Inc., accomplishes this by ducting the stale and moist air through a heat exchanger prior to exhausting it to the atmosphere (see drawing below). In the summer, this may be the cooled air, and in the winter, the warmed air. The fresh outside air is then drawn into the heat exchanger, circulated around the heated or cooled (depending upon the season) air and

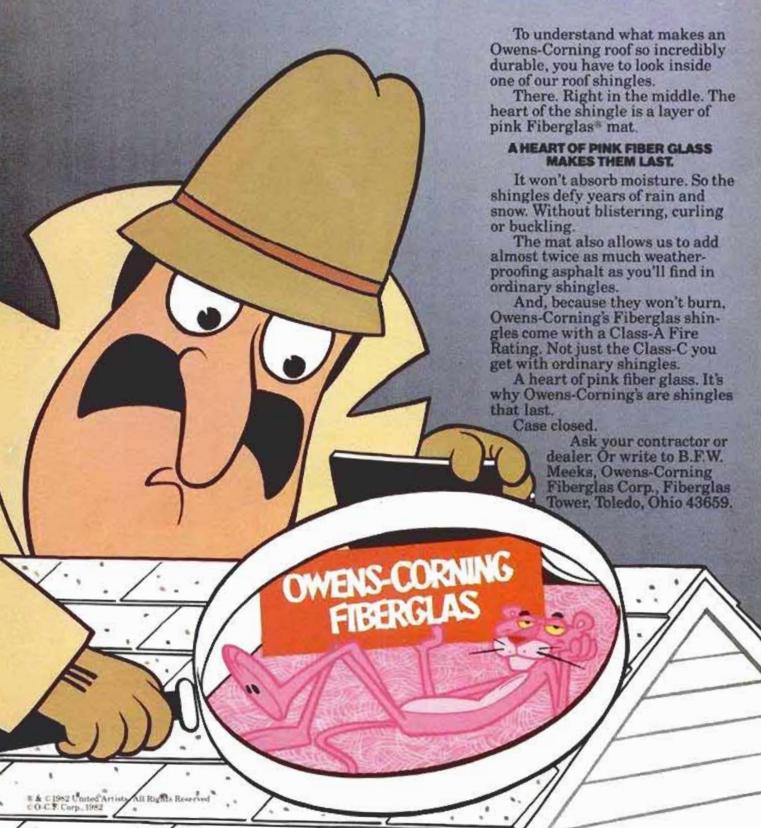
deposited into the house at temperatures very near those of the exhausted air. (The heat exchanger permits the two airstreams to be brought into close proximity, but doesn't allow them to mix.) The E-Z-Vent "transfers the heat or energy of outgoing air to the fresh air," as the company explains it.

You can install E-Z-Vent in the attic or crawl space of your home. It's approximately 14 × 16 × 62 in.; the unit contains a heat exchanger, two blowers and a filter, It sells for about \$500. For the name of the representative nearest you or for more information, write: Des Champs Laboratories Inc., Box 440, East Hanover, N.J. 07936



Outside air enters through the E-Z-Vent, is cooled or warmed, depending on the season, and then enters the room via the supply air duct. Stale, moist return (room) air enters the heat exchanger and passes to the atmosphere via the exhaust air duct.





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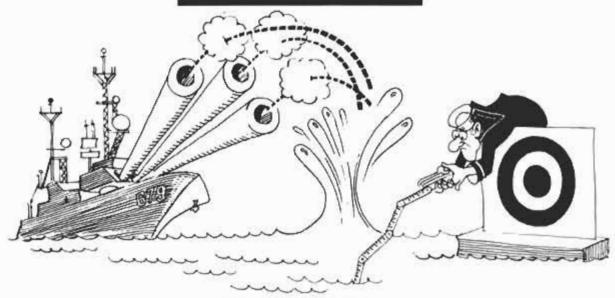
Wipe it off. No waiting. No hard buffing.



© 1982 Turtle Wax, Inc.



BRIEFS



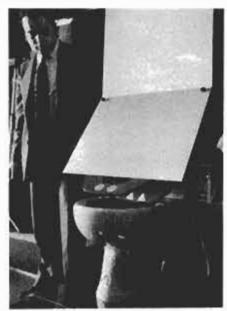
Great Guns

Fascinated by the information that a 2,700 pound projectile can go from zero to 1,700 mph within the 68 feet of a battleship's 16-inch gun barrel (see page 73), we had a question: How fast, we wondered, was that same shell going at the end of its 20-plus mile trajectory. With the Pentagon on the other end of the phone, we requested to know the 16-incher's "terminal velocity"—aware that proper

terminology is everything when approaching the military. The Navy said it would get back to us.

A few days later, word came that the "terminal velocity" of the shell is zero.

It turns out that the Navy measures "impact velocity" of a projectile at a theoretical point one foot from a target. For a 16-inch shell, it's about 1,100 mph.



PM's Whatsit

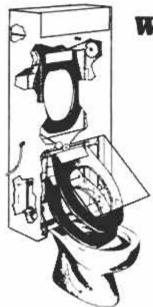
Given the culture's inclination for poetry in plumbing, you might easily guess at first sight that the device shown in this photo sprang from the brow of a Frenchman. Correct. In addition, what's happening is quite clearly indicated—all you have to do in this case is believe it. You don't? Turn the page.

Oh, the humanity...

Are you ready for Blimpboard? It's being billed as an *indoor* advertising medium, suitable (it says here) for arenas, gymnasiums, malls and schools. The blimp is 6 feet long and filled with 14 cu. ft. of helium (breathe easier, sports fans). It flies and maneuvers entirely by remote control, powered by batteries driving three gondola-mounted motors. Promotional messages can be silk-screened on the blimp's sides; or, a banner can be flown beneath it. And for getting out of those tight corners, it offers a unique function: It flies in reverse. Blimpboard is at 1636 North Woodland, Provo, Utah 84601.



BRIE



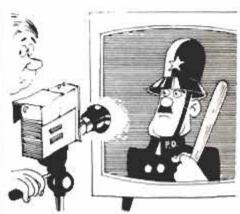
Whatsit revealed

What the Societe Française de Developpement et Distribution has wrought-and already has marketed to some European restaurants-is a public convenience which has three interchangeable seats and presents each user with one that has been newly cleaned and disinfected. A chain conveyor pulls the used seat into the cabinet, simultaneously positioning a fresh version. The entire process, including a rotary scrubber, is initiated and driven by normal water pressure, adding greatly to the context of the word, flush.



Poles apart

TechEx is a combination exhibit and seminar for technology exchange and licensing sponsored by an internationally known, Florida-based firm, Dvorkovitz & Associates, specialists in technology transfer. At this year's U.S. TechEx show, held recently in Atlanta, several East Bloc countries had scheduled exhibits. (Because of lack of manufacturing capacity, most of the Warsaw Pact nations, including the U.S.S.R., are active in licensing their technological developments and inventions.) They all showed up-except the contingent from Poland, which cancelled "because of circumstances beyond our control." The sponsors presented an exhibit anyway (photo): a black curtain-upon which descriptions of the Polish inventions that were to be shown were hung-plus a suitable floral arrangement.



Klystron Kops

Make way for the video police. As of a recent Appellate Court decision overturning a previous ruling in favor of Sony Corp., several retailers and one poor stiff who told the world he had bought videotape in order to record copyrighted material (films) on his VCR, it is illegal to tape movies at home. (See PM Electronics Monitor. page 10, Mar. '82.)

There is a petition before the Supreme Court asking it to hear an appeal of the appeal (unlikely-since the Court has agreed to hear only five copyright cases in its entire history). There is also Congressional legislation in the committee-hearing stage which would bring videotaping for home use under the Fair Use Doctrine-making

it licit.

But the movie industry, under association kingpin Jack Valenti, has found some Congressmen sympathetic to a bizarre proposal: that a surcharge be placed on both videotape machines and blank tape, paid ultimately by the consumer, for pooling and eventual dispersal to producers whose films are being copied.

The concept that innovative technology should cost us more just because it has leapfrogged conventional marketing structures is a hard one to swallow. And doesn't this "probable use" precedent invite, say, music publishers to tax Xerography supplies?

Through their industry association, the tape and VCR marketers are pursuing legal remedies and lobbying hard. But there may be a simpler solution: Every time a Hollywood producer turns out a movie that is unfit for home taping, for reasons of content or execution, he'll be assessed a sum to be pooled for return to owners of videotape equipment.



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CANADA AT ITS BEST

Light Smooth Imported Canadian Mist The whisky that's becoming America's lavorite Canadian



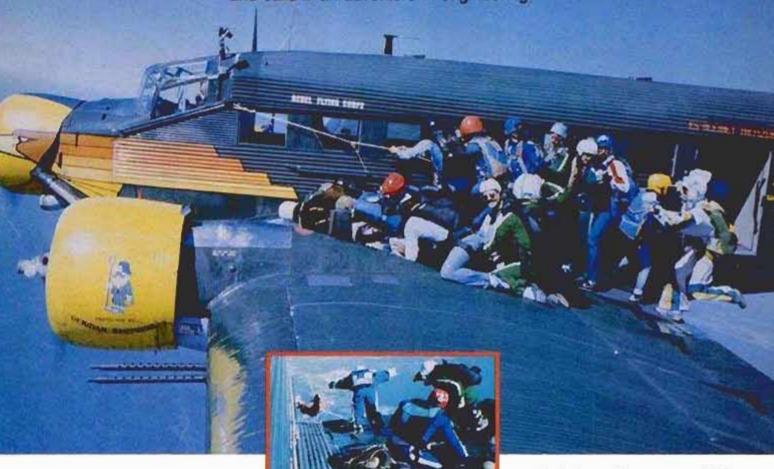
Share some tonight.

IMPORTED BY B-F SPIRITS LTD., N.Y., N.Y., CANADIAN WHISKY-A BLEND, 80 PROOF, © 1979.

Photographed at Bow Like, Province of Alberta, Canada.

Anatomy Of A Wing-Walk

Don't call it a stunt, says a pilot who put a world's record 19 souls on an airplane wing and calls it 'an adventure in engineering.'



Writer Martin Caidin has always been obsessed by airplanes, but his life took on another dimension when he acquired a vintage German Ju-52 transport several years ago. A frequent airshow participant, Caidin was looking for something new to do with his bird, dubbed "Iron Annie," when the idea of a recordsetting wing-walk came up. . .

PM Why did you do this stunt?
MC Don't call it a stunt. That's
the last thing it was.

PM Then what would you call it and why did you do it?

MC I'd call it an answer to a challenge. An engineering adventure. A dream for a pilot. A lot of people said it simply couldn't be done. That tells you what it was—a challenge, engineering, pilotwise and personal.

PM Describe how you prepared for the, well, let's call it the event.

MC It started a few months before we began getting ready to try for the world's record—14 people

Coed group of 19 'chutists clings to the ancient plane momentarily (top) before making quick but orderly exit into air.

outside an airplane. Around Christmas in 1980, U.S. Army sky diver Phil Rogge asked if I ever tried putting people out on the wing of the Ju-52. We'd done it a few times, but it was small stuff. So we decided to try it by putting people almost all the way out instead of hugging the fuse-lage?

PM How many people?

MC We figured seven on the wing and a couple atop the fuselage. Remember, we had no idea what would happen. We took off the left door, tied ropes all over the place and went up to 9,000 feet. I figured I'd better cut power so the prop blast wouldn't dump these guys off the wing. I was hitting about 115 mph when out they went on the wing and ... good-by lift! The control yoke in my hands went mad and jumped around and she rolled right over on her back.

PM That plane went inverted?

MC She rolled over on her back as pretty as you please, shaking like mad, and went down in a gentle rolling dive and got up to about 160 mph true airspeed. Those guys out there on the wing were hanging on and yelling like maniacs. At 5,000 feet, I hit power, they let go of the ropes, the shaking stopped and we pulled out. It was a terrific ride.

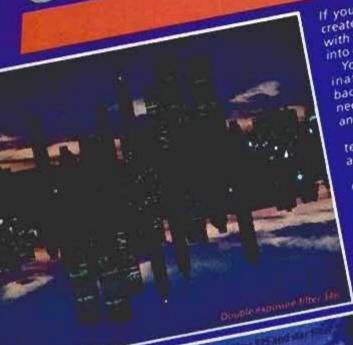
PM And that gave you the idea for the world's record?

MC It sure did. We figured if we could have seven on a wing and two atop the fuselage, without any power, we could fly with 18 people on the wings.

PM You said wings, plural. Did you try it that way?

MC We sure as hell tried. We (Please jum to page 67)

CHANGE THE WORLD. List of the Color Creative Filter System.



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You can travel anywhere your imagination will take you - and back with brilliant pictures. A need is the amazing Cokin System

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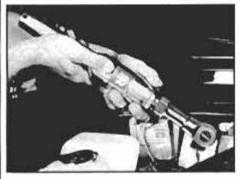
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THE PM GARAGE

CAPSULE REPORTS ON NEW AUTO PRODUCTS

Self-powered ratchet

You've seen the electric-powered wrench and the air-powered wrench. Now meet the battery-powered wrench. The Redi-Ratchet is supposed to spin threaded fasteners on and off all day on a three-hour charge to its NiCd battery pack. We didn't use it long enough to test that, but we did find it strong: The %-inch



Battery-operated Redi-Ratchet provides 20 ft.-ib. of torque. Free speed is 110 rpm.

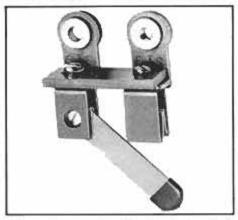
drive tool turns with a free speed of 110 rpm and supplies up to 20 footpounds of torque. The wrench can handle up to 100 foot-pounds of torque.

Weighing in at 3% pounds, the 14%-inch wrench uses a grease fitting to lube its reversible ratchet mechanism. A slim profile and 360-degree rotatable head help you work the tool in tight places.

The price is \$180 for ratchet and battery charger from Ingersoll-Rand, Dept. 50707, 253 East Washington Ave., Washington, N.J. 07882.—T.A.

Battery disconnect switch

If you're looking for a quick and easy way to temporarily disconnect your



Battery disconnect switch for side-terminal batteries will help prevent current drain.

car's battery, you'll find that using a battery disconnect switch is easier than removing battery cables.

Safety/Switch Model ST-9 for sideterminal batteries lets you cut the juice simply by flipping a knife switch.

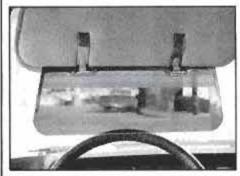
The unit is rated at 350 amps. continuous use, and uses cast brass and copper construction. We found it to be a well-made, heavy-duty switch, which accounts for its \$19.95 price tag.

Once the power is cut, you can work safely on your car's electrical system.

It's from Paragon Manufacturing Corp., 622 13th Avenue S., Hopkins, Minn, 55343.—T.A.

Clip-on sun visor

Driving into the blinding late afternoon sun is a hazardous condition easily dealt with, thanks to Yankee Products' foldaway sun visor. It's an oldie, but a goody. The smoked plastic shield mounts to the sun visor



Smoked plastic sun visor clips onto regular visor and folds away when not in use.

with two slide-on clips. No tools are needed. When not in use, the visor flips up and out of sight.

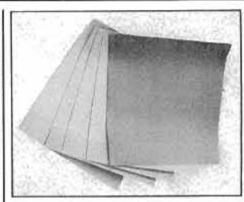
I found this Yankee product in the automotive section of a large department store for \$3. Yankee Products has a free catalog of its wares.

You can obtain one by writing to Box 910, 25 Grand St., Norwalk, Conn. 06852—Joel Breault

The right stuff

We've found that rubbing out new paint jobs requires a lot of muscle and a really fine-grit sandpaper. If you've got the muscle, Nikken's got a good waterproof sandpaper for either wet or dry use.

The silicon carbide closed coat is available in "C" weight 9 × 11-inch sheets ranging from 80 Type RRCC



Wet- or dry-use sandpaper uses siliconcarbide abrasive, comes in variety of grits.

grit to super-fine 1200 Type WTCC. Each sheet retails for about 75

Each sheet retails for about 75 cents and must be ordered in bulk. Contact Emgee Marketing Co., 5802 Tennessee, Clarendon Hills, Ill. 60514.—Joel Breault

Lifting the fog

The Tracer Chemical Corp., out of Tampa, Fla., has just begun selling its antifogging compound originally developed for NASA.

The TRX Anti-Fog Composition will eliminate fogging on both plastic and glass. It did a good job on my motorcycle helmet face shield and the car windshield, and also worked well on goggles and the bathroom mirror.

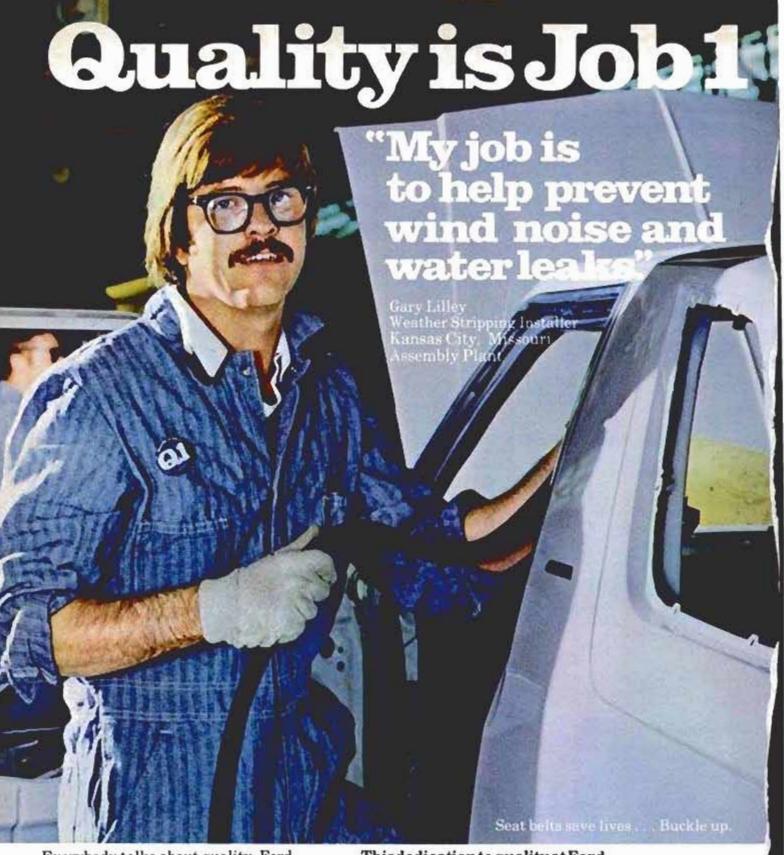
The small, 1-ounce bottle I tested is said to be good for 25 applications the size of the average auto wind-



Antifogging compound, originally made for NASA, stops fogging on glass and plastic.

shield. It costs about \$2.50 plus shipping, and you can order the product by writing directly to Tracer Chemical Corp., Box 7494, Tampa, Fla. 33673.

-Joel Breault



Everybody talks about quality, Ford people make it happen.

Our engineers create a design and specify materials that protect against wind noise and water leaks. People like Gary Lilley carefully fit weather stripping to provide a pleasing interior environment for driver and passengers in every Ford vehicle.

This dedication to quality at Ford Motor Company is paying off. Latest results show a 48% average improvement in quality over 1980 models as reported by new car owners.

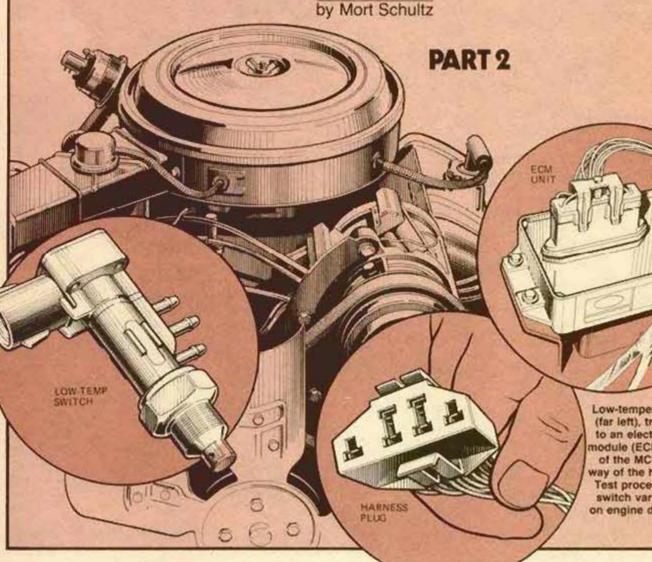
Visit a Ford or Lincoln-Mercury dealer and take a close look at what total employee, management, union and supplier involvement can achieve.

At Ford Motor Company, Quality is Job 1!



Ford Mercury Lincoln Ford Trucks Ford Tractors





Low-temperature switch (far left), transmits data to an electronic control module (ECM), computer of the MCU system, by way of the harness plug. Test procedures for the switch vary, depending on engine displacement.

f you don't have a copy of the April 1982 Saturday Mechanic on hand, rustle it up. You'll need the chart it contains that lists parts of the Microprocessor Control Unit (MCU), which is what Ford Motor Co. calls its computerized engine control system.

The April article (Part 1, Troubleshooting Ford's Microcomputer Control Unit, page 53) outlines which engines have the MCU. The MCU system regulates various engine functions and includes a self-diagnostic function. The system provides diagnostic messages in the form of numerical service codes, presented as intermittent voltages. The service codes reveal whether the MCU system is working properly; if it's not, the codes locate the trouble area. And the self-diagnosing feature of MCU makes troubleshooting

a job you can do yourself.

The purpose of troubleshooting the MCU is to determine if a part of the system is causing poor engine performance. To check the MCU, you'll need one of two instruments: the Self-Test Automatic Readout (STAR) unit or an analog voltme-

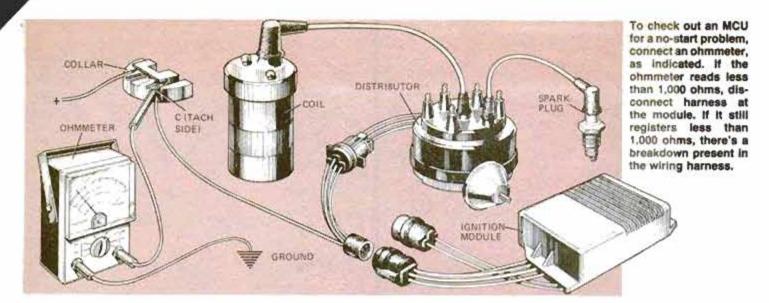
STAR (part No. 07-0004) is about \$125 from Ford/Mercury dealers or parts and accessories stores that sell Rotunda products. The STAR displays two-digit service codes to let you know the source of a problem.

If you choose to use an analog voltmeter instead of the STAR, it should have a 0 to 20-volt d.c. scale. You'll also need a jumper wire.

With a voltmeter, you have to count pulses of the voltmeter needle to read the service codes. Interpre-

ting this takes practice.

The first pulse or series of pulses is equivalent to the first digit of the two-digit code a STAR unit would display. Then there is a pause, followed by another pulse or series of



pulses, which is equivalent to the second digit of the two-digit code STAR would display.

If you use STAR, an "11" is displayed to let you know the system is okay. A voltmeter registers an "11" when the needle rises for 1/2 second, drops to 0 for 2 seconds, then, rises for another 1/2 second. To verify the reading, keep the voltmeter in place. After a pause of 4 seconds (needle on 0), the cycle will repeat itself.

Take another example. A STAR display of "42" indicates that a rich fuel mixture is affecting engine performance. The trouble could be a sticking choke, high resistance in a fuel-control-solenoid circuit or a malfunctioning exhaust-gas oxygen sensor. The voltmeter would display a code 42 in the manner shown in the illustration on page 36.

Before testing the MCU

Before you begin the MCU diagnostic check, make sure all other potential sources of poor engine performance have been eliminated. This includes general electrical problems, vacuum leaks, ignition component malfunctions, problems in carburetor systems not controlled by the MCU, various emissions-control part malfunctions and internal engine problems.

If problems persist, proceed to troubleshoot the MCU. Warm up the engine to normal operating temperature-that is, until the upper radiator hose feels hot. Then, shut off the engine, place the transmission in PARK or NEUTRAL, engage the parking brake and make sure all accessories are off.

There are two conditions that can upset this procedure: 1. The engine won't start; 2. The engine problem you're trying to solve occurs only when the engine is cold. In both cases, you can't use the MCU selftest feature, but must try a different troubleshooting procedure.

Engine won't start

The only way the MCU can keep an engine from starting is if the TACH input circuit to the MCU computer is shorted to ground. This causes current to bypass the ignition coil primary. If the ignition coil isn't getting current, neither will sparkplugs. The engine, therefore, can't start.

If a no-start condition exists and all conventional causes have been ruled out, there might be a short. The short could be in the wiring harness from the ignition control module or in the MCU computer. To discover and locate a short do the following:

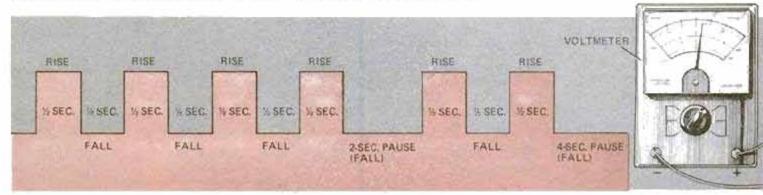
Disconnect the horseshoeshaped connector from the top of the coll. Disconnect the plug that attaches the ignition control module to the horseshoe connector and the distributor. Then measure resistance with an ohmmeter between the terminal on the horseshoe connector marked TACH and ground.

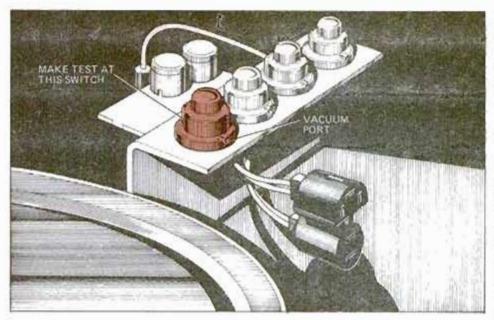
2. If the resistance is greater than 1,000 ohms, the MCU system is not the cause of the no-start condition. If resistance is less than 1,000 ohms, disconnect the MCU module from the wiring harness and measure resistance again. If the resistance measures less than 1,000 ohms, the fault is somewhere in the circuit that Ford refers to as circuit 11 in its wiring-diagram manual. If the resistance is greater than 1,000 ohms with the MCU module disconnected from the wiring circuit (but less than 1,000 when connected), the MCU control module is defective.

You can't repair the MCU control module. It must be replaced, but remember that the MCU system, including the control module. carries a 50,000-mile/five-year warranty, whichever comes first. However, if you go back to the dealer, don't tell him you've been troubleshooting the MCU system: He can probably void the warranty.

Handling cold-engine problems

All MCU systems have a low-temperature switch that sends signals





Low-temperature switch on 4.9-liter engine uses vacuum and electricity to signal the MCU. Apply 4 inches of vacuum to the port; take an ohmmeter test at the switch shown.

to the computer. There is a preliminary test for the four- and six-cylinder low-temperature switches that can be performed before the diagnostic test. However, the test sequence can also refer you to the low-temperature switch.

Therefore, if a cold-engine performance problem exists and all conventional causes have been ruled out, the low-temperature switch can be tested. Each size engine requires a different testing procedure, since each uses a different type of low-temperature switch. Test the eight-cylinder switch during the diagnostic sequence.

■ 2.3-liter engine: This engine utilizes an electrical low-temperature switch. With the engine off and cold (coolant temperature has to be below 95° F.), remove the switch from the engine and connect an ohmmeter across its terminals. If the meter shows switch contacts are closed (meter will read less than 5 ohms), the switch is bad. Replace it.

Note: Cold-temperature switches are supposed to open when the engine is cold and close when the engine is hot.

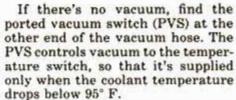
■ 4.9-liter engine: The low-tempera-

ture switch of this engine uses both vacuum and electricity. With the engine off and cold (not over 95° F.), disconnect the electrical harness and vacuum hose from the switch. Use a hand vacuum pump to apply at least 4 in. Hg of vacuum to the switch through the vacuum port. As you do this, check across the switch terminals with an ohmmeter. If the ohmmeter shows less than 5 ohms, replace the switch.

If the switch shows an open condition (more than 5 ohms), which it should, start the engine and hold your thumb over the opening of the vacuum hose. (The engine coolant temperature should be below 95° F.) If you feel a strong pull, the low-temperature switch and its vacuum system are okay.

To test the low-temperature switch on V8 engines (right), turn engine off, disconnect the switch and test the resistance across the terminals. You should tap a knock sensor with a light hammer in order to begin the MCU testing cycle.

Attach voltmeter and jumper wire to trigger the MCU system into the self-test mode. The voltmeter needle will begin to pulse, as indicated in the illustration. This particular example indicates a code 42 (four pulses, a short pause, followed by two pulses).



Disconnect the vacuum hose that extends from the engine to the PVS. With the engine running, check for vacuum to the PVS by holding your finger over the end of the hose. If there is vacuum, the PVS is bad. Replace it. If there's no vacuum, check the vacuum hose from the engine to the PVS for a leak or blockage.

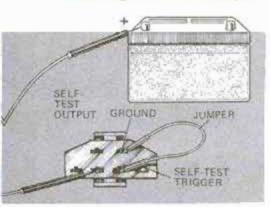
■ V8 engine: This engine uses two low-temperature switches that are activated electrically. One should open when engine coolant is below 128° F. The other switch should open when the engine coolant is below 55° F. Those switches are tested during the MCU "functional" or diagnostic test.

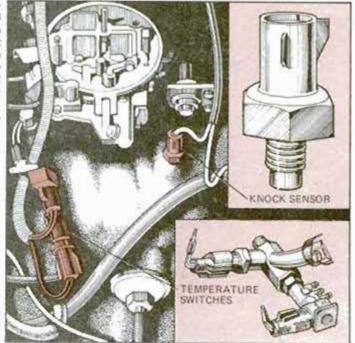
Connecting test equipment

To connect the STAR, keep the ignition off and attach the ground (black) lead to the battery negative terminal, test output (red) to the output terminal of the MCU computer self-test connector, and test trigger (white) lead to the self-test trigger terminal of the self-test connector. The self-test connector is a plug attached to the MCU computer (electronic control module).

Once the STAR displays a service code, it will keep showing it even if you disconnect the unit from the vehicle. To erase a code, press the push button or turn off the power switch.

The STAR indicates when its bat-





tery has to be replaced. When the STAR is turned off, LO BAT will appear briefly in the upper left-hand corner of the display window. This is normal. However, if LO BAT stays on, turn off the power switch and replace the 9-volt battery.

To use an analog voltmeter rather than the STAR, make sure the car ignition switch is off. Connect a jumper wire between the self-test trigger and ground of the self-test connector by clipping the jumper alligator clips between two of the terminals you can see in the self-test connector plug.

Hook up the voltmeter (+) lead to the battery positive post, and the voltmeter (-) lead to the self-test output of the self-test connector. Set the voltmeter d.c.-volt selector switch to the 0 to 20 d.c. scale.

If you're testing the MCU system of a four- or six-cylinder engine using a STAR or voltmeter, disable the canister purge valve of the fuel evaporation control system to prevent a wrong signal to the computer, indicating that the engine is operating on a rich fuel mixture. To do this, disconnect the charcoal canister hose from the canister purge valve and plug it with a golf tee or pencil.

If you're testing the MCU of a V8 engine, remove the PCV valve from the valve cover to prevent a wrong signal to the computer when the engine is warmed up, indicating that the fuel mixture is too rich.

On V8 engines equipped with a vacuum delay valve (a wafershaped, two-color valve that is spliced into a vacuum line in the airpump control system), there's a tee and restrictor in the thermactor vacuum control line. The restrictor must be uncapped during the functional test.

Getting test grades

To perform the diagnostic test on a V8, perform the following:

1. Make sure the engine is fully warmed and the upper radiator hose is hot and pressurized. Connect the STAR or your analog voltmeter as described. Turn the STAR on, it should flash "88," then "00." Press the push button. Run the engine at 2,000 rpm for 2 minutes, then shut it off. Restart the engine immediately and allow it to idle.

2. If you're working with the STAR, a code 40 should appear. If you're working with a voltmeter, the needle will pulse four times. The throttle kicker will increase the idle speed. Either of these is the initialization code. On cars equipped with a knock sensor, place a %-in. extension on the manifold near the base of the knock sensor. Tap the end of the extension lightly with a small hammer for 15 seconds as soon as the initialization

3. After the four initial pulses or code 40 appears, record voltmeter pulses (transcribed into service codes) or the STAR service codes. Within 90 seconds, the throttle kicker will retract. If a code 11 is displayed, the MCU system is okay. For vehicles equipped with universal ignition modules, however, Ford prescribes additional tests if the vehicle has

code appears.

CANISTER

To disable the canister purge valve, disconnect it and then plug the hose at the canister or at the valve.

PURGE VALVE

TO FUEL

TANK

detonation problems at wide open throttle and/or poor performance.

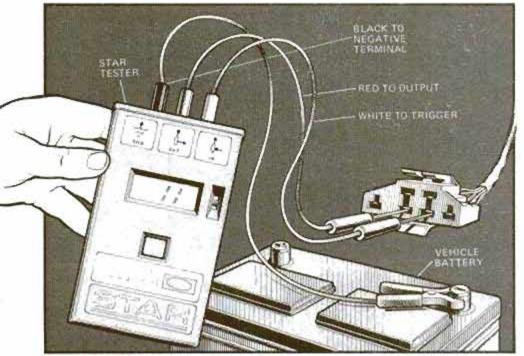
To perform the diagnostic test on a four or six-cylinder:

1. Make sure the engine is fully warmed. Connect the STAR unit or analog voltmeter as indicated. Turn the unit on. It should flash "88," then record a steady "00." To receive service codes, press the push button. Then turn the ignition key from off to on, without starting the engine. One quick initialization pulse may occur (code 10) immediately after the key is turned on. The service code display will occur after 5 seconds. If you get anything other than a code 11, perform additional tests.

2. If a code 11 appears, start or restart the engine. Increase engine speed to 3,000 rpm and watch for a code 20 or 30 on the STAR, two or three pulses on a voltmeter. If code 11 reappears, the basic MCU system is sound, however Ford recommends additional component tests. If a malfunction code appears, additional subtests are necessary.

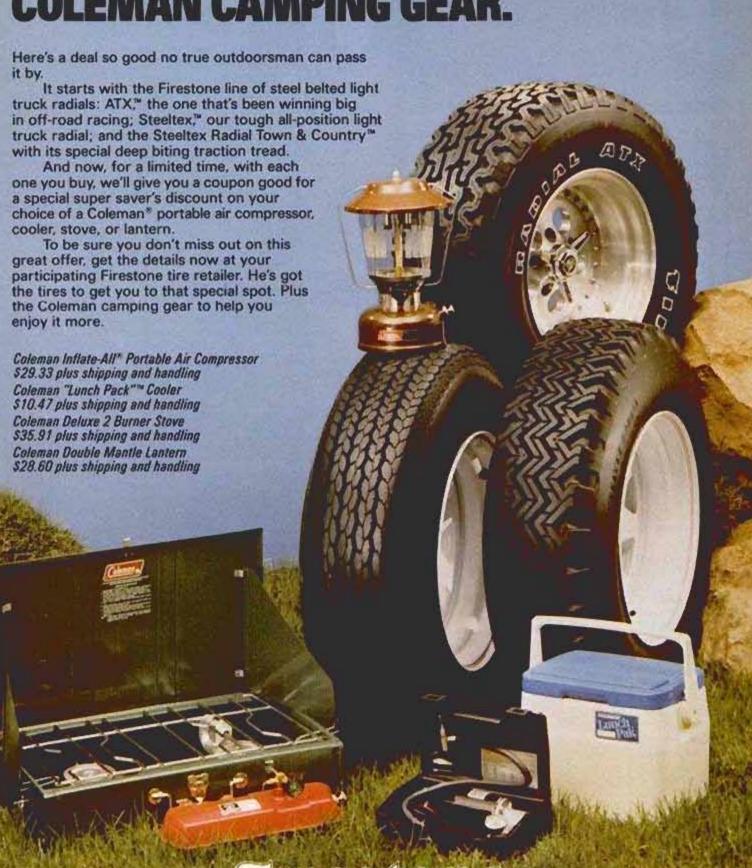
The chart in the April '82 issue outlines service codes and what components of MCU may be involved.

Note: In a case where a code or pulses indicate a malfunction, a different code or number of pulses may show itself during the "repeat" cycle. This indicates that more than one malfunction exists. Furthermore, during four- and six-cylinder tests, if you get a code 11 during fast (or slow) idle, but a different code during slow (or fast) idle, there's a malfunction. The only time you don't have a problem in the MCU system is when code 11 or pulses equivalent to code 11 appear during both the fast- and slow-idle phases of the test.



The STAR tester gives you digital readouts of the service codes. It costs about \$125 from a Ford dealer. It reduces the chance of error of the pulsed voltmeter technique.

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Living in the past

My 1981 Dodge Aries (2.2-liter engine and automatic transmission) is a pleasure to drive, except for one thing. Upon starting out, the car hesitates so badly it nearly conks out. Pushing the accelerator pedal down makes it suddenly catch hold. The dealer says he replaced the accelerator pump in the carburetor, but the engine still stumbles badly. Any ideas?—Don Lauderback, Hickory, Pa.

Faulty accelerator pumps are a major cause of hesitation, but not the only cause. There are a number of other possible conditions that can cause initial stumble. They include a sticking secondary throttle valve, a dirty idle-transfer port, or even an incorrect idle-speed or idle-mixture adjustment. If the problem occurs only when the engine is cold, the EGR system may be malfunctioning.

I would, therefore, ask the dealer to get out his 1981 Engine Performance Manual for Aries, Reliant, Omni and Horizon. It explains how to diagnose hesitation problems. Note, in particular, the chart on page 14-6 of the manual. It explains what to do if the hesitation occurs only when the engine is warm, or under both warm and cold conditions.

There's another publication that can help your dealer get to the bottom of this and other driving problems exhibited by both 2.2- and 1.7-liter engines. It's called Driveability Test Procedure, 1982 Electronic Feedback Carburetor System. The booklet provides a step-by-step troubleshooting procedure for every component that can conceivably cause the kind of driving problem you're having.

Licking clicking

Not long after purchasing a new 1978 Chevrolet Malibu station wagon, I noticed a faint clicking from the wheels that increased and decreased in intensity with speed. The Chevrolet dealer said he couldn't hear the noise, so we took the car to an independent brake mechanic who turned the rear-wheel drums on a lathe, thinking that perhaps they were outof-round. There seemed to be some improvement, but the noise is again as loud as ever. Our brakeman now says the front-wheel rotors are wearing heavily on the side closest to the axle.

The Chevrolet dealer says this is normal and that rotors should be turned when I have new pads installed. I understand new rotors cost \$80 each. Is there anything I can do to correct this condition and avoid the expense of new rotors? Do you know of any remedy for the clicking noise?—Wesley R. Kimbell, West Covina, Calif.

It seems that you are concerned about two separate problems: rotor wear and a clicking noise. I'll assume that they're not related, in that you seem to have determined that the noise is coming from the rear.

First, why do you assume your car needs new rotors? Maybe it does, and maybe it doesn't. Your dealer said rotors would have to be turned—not replaced.

I agree with the dealer that it's normal for rotors to wear more heavily on the axle side. When braking, the pressure exerted by the pistons is on the inner brake pads,

which apply on the inside (axle) part of the rotors.

Rotors can be turned and reused as long as the minimum-thickness dimension embossed into them is not exceeded.

For example, if the refinished dimension found on a rotor calls for a minimum thickness of 1.230 inches, and your rotor after refinishing is 1.230 inches or thicker, keep the rotor in use. But if the thickness after turning is 1.215 inches, replace the rotor.

However, due to uneven wear, some rotors have maximum machining specs for each face, in addition to a minimum thickness spec.

In respect to the clicking noise, the fact that it diminished when the rear drums were turned indicates that the brake mechanic was on the right track.

It's quite possible that one of your rear axles is slightly bent. This might cause the drum to contact the rear shoes in such a way as to create the clicking noise. When the mechanic reinstalled the machined drums, he probably loosened up the brake adjustment, which caused the noise to be diminished. Another possibility is a bad rear axle bearing. A faulty rear brake component is a third candidate.

Biting the hand . . .

The front wheels of my Volkswagen Rabbit diesel are extremely difficult to turn when the car is standing still and just plain difficult when underway. My ungrateful little rascal has struck back at me several times, leaving me with fingers that felt as if they were gnawed by a dog. Assuming a power steering unit would solve this problem, I'm wondering if it would wipe out whatever engine power my airconditioned Rabbit now possesses. Can you tell me?—Norman Williamson, Jr., Claremont, Calif.

Probably, if you don't have a 1982

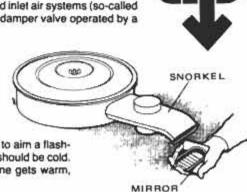
(Please turn to page 42)

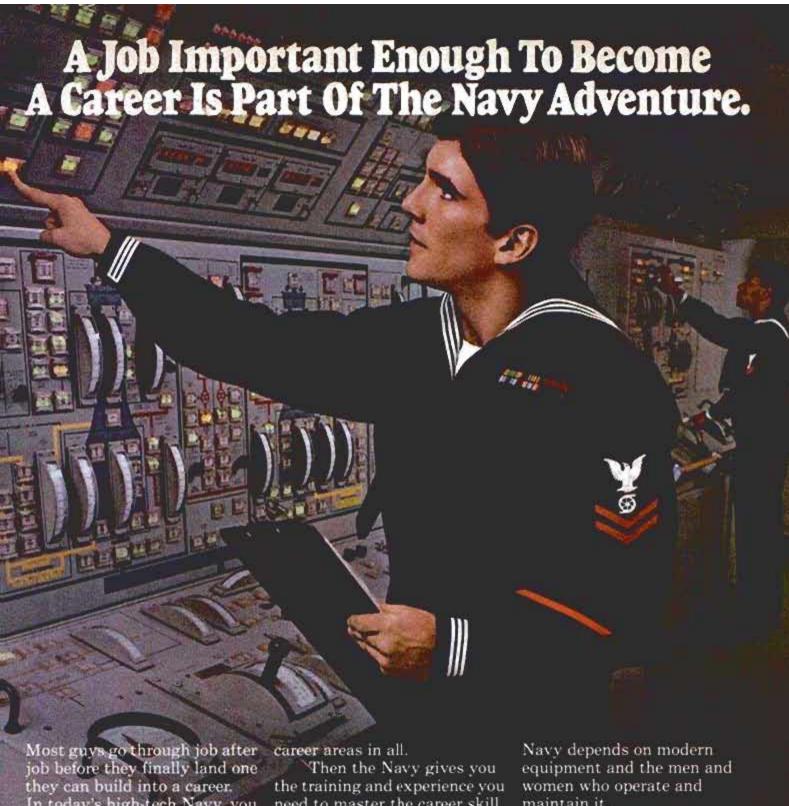
Say ahh

The number of complaints about cold-engine problems—stalling, surging and hesitation—has risen sharply since early in the 1970s when manufacturers started equipping cars with preheated inlet air systems (so-called thermostatic air clearers). This system has a damper valve operated by a

vacuum motor which, in turn, is controlled by a temperature sensor in the air cleaner. By providing warm air, it allows a cold engine to run at a leaner choke setting than it otherwise would. When you check this system, you may have trouble seeing the damper valve, which is buried deep inside the throat of the air-cleaner snorkel.

A solution is to hold an old rear-view mirror at the mouth of the snorkel and, if necessary, to aim a flashlight down the throat. Start the engine, which should be cold. The damper valve should close. As the engine gets warm, the valve should open slowly.





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CAR CLINIC

(Continued from page 40)

model. From the sound of your letter, I don't think you do.

Volkswagen did not equip pre-1982 Rabbits with power steering. Neither does it have a retrofit power steering kit for pre-1982 models, diesel or gasoline.

For 1982, VW is offering power steering as an option. However, the sizes of Rabbit engines have been increased to compensate for the added load. Pre-1981 Rabbit diesel engines are 1,471 cc in size, but the 1981 Rabbit diesel is 1,588 cc. The 1981 Rabbit gasoline engine is 1,715 cc. Previous-year models with gasoline engines have either 1,457-cc, 1,471-cc or 1,588-cc engines, depending on the year.

Make sure your tires are fully inflated. Underinflated tires can make the steering "heavier" than it should be. Tires that are wider than original equipment can produce the same result.

Ins and outs

On five occasions over the last two months, the 5.7-liter V8 diesel engine of my 1980 Oldsmobile Delta 88 has died on me while traveling on a level road at a speed of about 20 mph. There's never been a problem getting started again. At higher speeds, the engine performs flawlessly. I've changed the fuel filter, but it hasn't helped.

The car had been driven only 6,700 miles when the trouble started. What could be the cause?—Harry Westerberg, Portland, Ore.

Since I received your letter toward the end of winter, I would say that the most likely cause of the condition that you describe is fuel wax-

Although this condition generally causes hard starting, it frequently results in the kind of performance you describe when a garage-kept car is driven in cold weather. Is your car equipped with the fuel-heater option? If not, it should be in an area where the temperature drops below 20° F. It's also important that you use winterized fuel.

Another possibility is that the mechanical fuel-delivery pump, which looks just like a conventional gas-engine fuel pump, has failed. It can be pressure-tested in the same manner as the conventional pump.

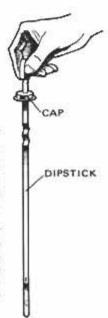
There's also a chance that the cause of your trouble is inside the fuel-injection pump with a sticking

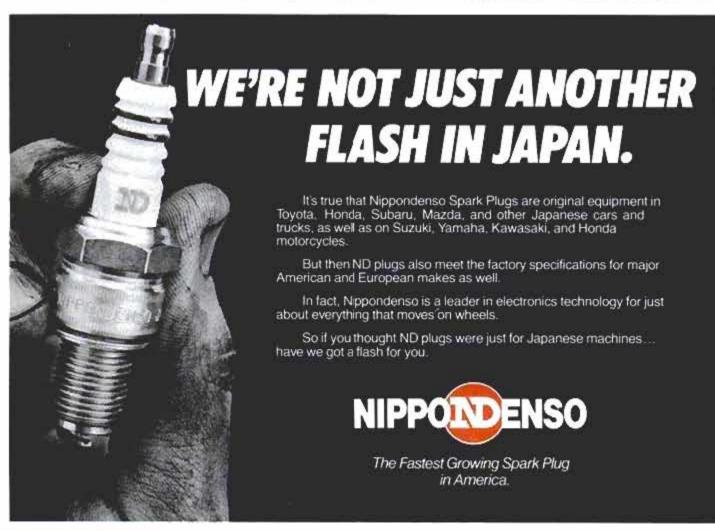
Hat trick

If you get a higher-thannormal fluid level on a dipstick, maybe it's flipped its
lid. Suppose the engine oil
level reads "overfilled" after an
oil change, when you know the
engine's been filled with the amount of oil speclified by the manufacturer. Check the retaining

cap on the dipstick. It may have come loose and been pushed up as you shoved the dipstick into the tube. This will cause the dipstick to sink deeper into the oil than it should, and the level will read higher than it really is. To repair this, position the dipstick so the oil level reads FULL. Slide the cap down until it rests on the tube, remove the dipstick as you hold the cap steady and braze or weld the cap to the dipstick.

Make this repair after changing the oil and the oil filter and running the engine a while when the correct amount of oil is in the engine and the filter is already filled.







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"I had dreamed of retiring for years, but was afraid to quit my salaried job. I had never used this to learn. I sharpened 30 blades my first week my life, I can say that I am content.

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William J. Mitchell, Tempe, Arizona

"I have had excellent results with the Sharp-All . . you just can't beat it ... it is easy to set-up, doesn't take long to change stones . . . all in all, it is no trouble to work with. LaVern Heim, Canton, Kansas

your own if you want it ... Yesterday, I made \$32.00 in two hours." Dave R. Lewis, Hoguism, Washington

type of equipment, but the Sharp-All was real easy without advertising at all. Now, for the first time in

> vacuum lock in a gasoline engine. Your diesel starves for fuel and may stall, be hard to start or emit excessive amounts of smoke. How it reacts depends on how large the leak is and at what speed the engine is being

It's also quite possible that general fuel-system contamination is causing your low-speed stall. If so, the tank will have to be removed, drained and cleaned.

GOT A PROBLEM WITH YOUR CAR?

Just ask Mort about it. Send your question to the Car Clinic, Popular Mechanics, 224 West 57th St., New York, N.Y. 10019. While letters cannot be answered individually, problems that are of general interest will be published in the column.

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FIND OUT ALL ABOUT IT No Obligation -No Salesman will Call



SERVICE TIPS

According to American Motors, there's

something new to worry about if you acciden-

tally reverse the battery cables. You can dam-

age the fuel-feedback module in 1980-82 mod-

els, as well as the alternator diodes and radio.

For years, you've heard not to tighten a new

oil filter with a wrench. That's changed now for

new GM engines using the small 75-mm oil

filters, of which the PF 40, PF 45, PF 47 and PF

51 are the most common. If you can't hand-

tighten the filter the number of turns specified

So be sure to get them straight.

No. 4105671) to stop the loss.

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The air leak could be located at the injection-pump suction line, at the inlet fitting of the fuel filter, at any of the fuel-line clamps or at any one of a number of other places. Determine if there is a leak by splicing a short piece of clear plastic hose into the return line where it leaves the injection pump. Start the engine. If you see foam or air bubbles in the fuel flowing through the plastic hose, there is an air leak. To find the leak, examine all connections for moistness. Check the condition of all clamps and hoses.

on the oil-filter label, use an oil-filter wrench. Engine oil leaking from the rear of the intake manifold of a 1980-82 Chrysler Corp. 318- or 360-cu.-in, engine is probably caused by a rear gasket that's slipped from between the manifold and block. Service bulletin 09-04-82 (1/25/82) tells your dealer how to use RTV sealer (part No. 4026070) and a retainer (part

If you get a miss when you blow the horn of a 1982 GM 2.5-liter, four-cylinder engine with electronic fuel injection built before November 1981, have a .25-.5-mfd capacitor (part No. 1851428) installed between the horn feed wire and ground at the horn connection. Voltage feedback from the horn coil when the horn toots results in irregular injection-unit pulses. Each GM division has issued a service bulletin about this. For instance, the Oldsmobile bulletin is No. 82-T-3.

metering valve or a weak governor weight retainer. If so, the pump has to be disassembled to clean or replace the metering valve or to replace the governor unit. This job should be done by a technician trained in handling Roosa Master pumps, which are manufactured by Stanadyne Inc.

However, before getting involved in pump overhaul, there are three more "exterior" conditions that should be checked.

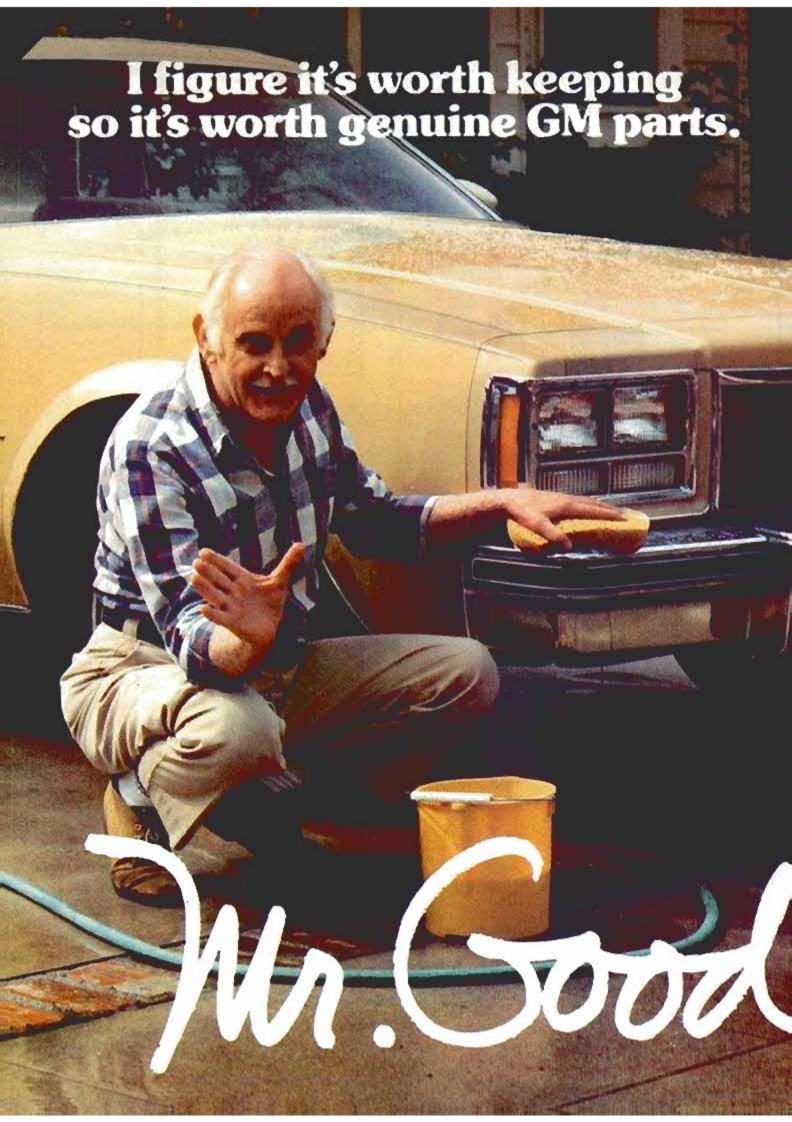
One of them concerns the fuelreturn line which comes off the fuelinjection pump and goes to the fuel tank. This line helps maintain a specific differential pressure inside the pump, which is necessary to maintain the right amount of fuel deliv-

If the fuel-return line is partially plugged, the differential pressure is thrown out of whack, upsetting correct fuel delivery.

Another condition that may cause low-speed stalling is an air leak into the fuel system.

The slightest leak in the diesel fuel system-even one so slight that fuel won't drip through to reveal the leak-will draw air into the system to displace fuel. When this happens, a condition occurs that is similar to

JUNE 1982 43



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APPLIANCE CLINIC QUESTIONS ANSWERED

Plugged up

My problem concerns the automatic ice maker on my Sears refrigerator-freezer, model No. 106-7620661, purchased in 1963. The ice maker worked fine for four or five years, until it suddenly refused to turn out any more ice cubes. I checked every component in the ice cube unit, the water valve and solenoid, but could find no fault

As a last resort, I removed the water line; at the point where it passes through the back of the box, there was an ice plug preventing water from flowing into the ice maker. All efforts have failed to produce a permanent solution. Your comments and suggestions would be most appreciated.—Stanton Draper, Carmichael, Calif.

Sounds as if the water-fill valve for your ice maker is causing your problem. There's a possibility that the valve is not seating properly. If a few drops of water leak through the valve when they shouldn't, these drops could freeze at the point where the tubing enters the cold freezer temperature. I suggest replacing the water valve as a complete assembly. The part is No. 627558; it costs approximately \$14 at any Sears store.

Blanket blues

Can you provide some information on how to repair an electric blanket that won't heat anymore? If home repair is impractical, I want to buy a new one now, during the summer sales. Thanks for your help.—John Wendling, Chicago

If you think that your blanket isn't heating, try this test: Fold the blanket as it would be in the box or package. Connect the control thermostat to the blanket and to a wall outlet. Next, switch the blanket on and set the dial to HIGH. Wait 10 to 15 minutes. Put your hand between the folds of the blanket; you should feel warmth.

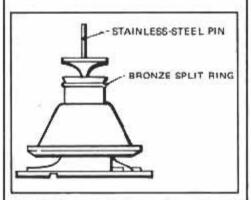
If the blanket is cold, you will need to take it to an authorized service repair facility or buy a new one. It isn't a great idea to make home repairs on the blanket or control. Electric blankets are made according to Underwriters Laboratories' (UL) standards and the electrical components are sealed inside the blanket. Also, the thermostat which controls the amount of heat given off by the blanket is calibrated when

the dial is assembled at the factory.

Tired dishwasher

I have a KitchenAid dishwasher, Model KDS-17. During the wash cycle, dishes aren't cleaned satisfactorily. The soap in the soap dispenser is only partially dissolved. The water temperature is 150° F. Could it be that the pump is worn and not pumping the water with sufficient force to remove all the food particles from the dishes?—Robert H. Artman, Lakeland, Fla.

Sounds like your problem may be in one of several areas: the wash arm (a four-way hydro sweep), the wash-arm support, the water-fill



The stainless-steel pin on the wash-arm support should remain fast to the support; the bronze split ring should be intact.

valve or the drain valve. First, remove the wash arm. Check the spray holes for any blockage. Turn the wash arm over. Check the area inside at the point that the wash arm attaches to the wash-arm support. Look for any deep grooves in the black plastic. Deep grooves can cause a drop in pump pressure being delivered to the wash arm.

Next, check the wash-arm support (see drawing). The support looks like a black tower; it's located under the wash arm. Check the stainless-steel pin which protrudes from the top of the support. You shouldn't be able to turn or move this pin up or down. Look at the bronze split ring below the pin. Check for cracks or possibly a broken ring. If you notice anything wrong with the wash-arm support, you can buy a new one. The support

If you have a question about any appliance, send it along with the model and senal numbers, to Appliance Clinic, Popular Mechanics, 224 West 57th St. New York, N.Y., 10019. Sorry, but letters cannot be answered individually. Problems of wide interest will be discussed in this column.

comes as one assembly for replacement; it's part No. B111431.

Now, check the water level in the tub. Start the dishwasher. Listen for the machine to fill. As soon as the fill cycle stops, open the door. Check the water level. It should be about ¾ in. below the bottom of the wash-arm hub. If there isn't enough water in the tub, look for a restricted water-fill valve or a leaking drain valve.

SERVICE TIPS

Saving energy is as important in summer as it is in winter. That's why you should get your airconditioner ready for those hot days ahead with a little maintenance now. Here are some tips from Research Products Corp., makers of Ezkleen room airconditioner filters:

If you noticed any mechanical problems toward the end of last summer, have the unit checked by a service specialist; otherwise, basic maintenance will be sufficient.

Airconditioners need a strong, steady flow of air to perform properly. Filters are used to trap pollen, dust and dirt, so you have clean air at all times. If the filter becomes clogged, airflow is reduced and you won't get maximum performance from your unit.

■ Cleaning your airconditioner is easy. Remove the front panel. Using a vacuum cleaner, work inside the wails, louvers in the front panel and inlet louvers. You may also want to wipe the inside with a solution of water and mild detergent.

■ Next, check the air filter. If it's made of aluminum, wash it and recoat it with filter adhesive. It can be reused for many years. If it's made of nonwashable material, replace it when it becomes dirty.

When you start using your airconditioner, keep the following suggestions in mind:

 Use your airconditioner according to current energy recommendations suggested by your local utility.

2. If possible, turn on the airconditioner before the room heats up.

Keep windows and doors closed to prevent escape of cold air.

Use shades, awnings and drapes to keep direct sunlight out.

Keep the grille and register open for proper alignment.

Check and clean the air filter periodically.



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Peerless developed a way to drop in the entire assembly and tighten it—by hand. Then you just connect the supply lines with special quick-connectors—by hand. And you're done.

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PM WORKBENCH PM EXAMINES PRODUCTS AND SERVICES

Durable finish

A well-known maker of quality wood materials—Deft finishing Inc., 17451 Von Karman Ave., Irvine, Calif. 92714-has introduced a new product called Deftco Danish Oil Finish. It is claimed to successfully combine the superior penetrating qualities of tung oil with the durabil-

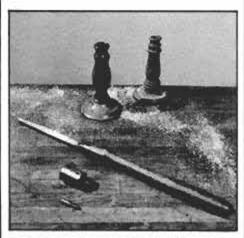


New finish is sold in 13-oz. aerosol, pint, quart and gallon cans at these suggested prices: \$3.49, \$4.94, \$6.60 and \$19.99.

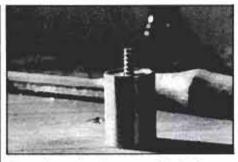
ity of polyurethane finishes. At paint and hardware stores, it comes in three shades of walnut and a natural wood tone .- S.W.

Tools for master woodturners

For the past six months I have had the joy of working with two beautiful woodturning tools. The first, a %4-in.-diameter turning gouge, has to be the best gouge I've used. Crafted of M2 tool steel, it's hardened to RC62 and sharpened to a fingernail shape. It comes with a 243/4in. Brazilian hardwood handle for



A lathe gouge and screw chuck for serious woodturners were tried by PM shop editor.

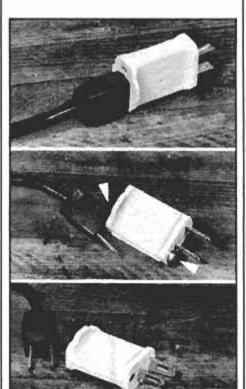


Screw chuck for those who build their own chucks has exceptional holding power.

\$69.50 postpaid. The Screw Chuck is machined out of stainless steel and boasts knife-edge-shaped threads for good end-grain holding power. Available for Rockwell, Shopsmith and Craftsman lathes, it's \$59.50 postpaid from International Woodworking Equipment Corp., 11665 Coley River Circle, Fountain Valley, Calif. 92708.-H.W.

A short fuse

For added protection against the dangers of circuit overload and short circuits, install some Nobility Safety Fused Plugs, Designed for



Fused plug gives added protection against overloads and short circuits. Replacing blown fuse takes seconds: Unplug from outlet (top), dislodge fuse with one plug blade (middle), pull out fuse and discard (above). Insert new fuse, push to back.

use with extension cords, these simple devices come with 7- or 10-amp. fuses that will tell you right away if you're putting unsafe demands on a particular circuit. Each plug is about \$1.50 at hardware stores. Made by New York Merchandise Co. Inc., 32 West 23rd St., New York, N.Y. 10010.—S.W.

Squeezable P-trap

The Squeeze-N-Flow is a new flexible plumbing trap with several good



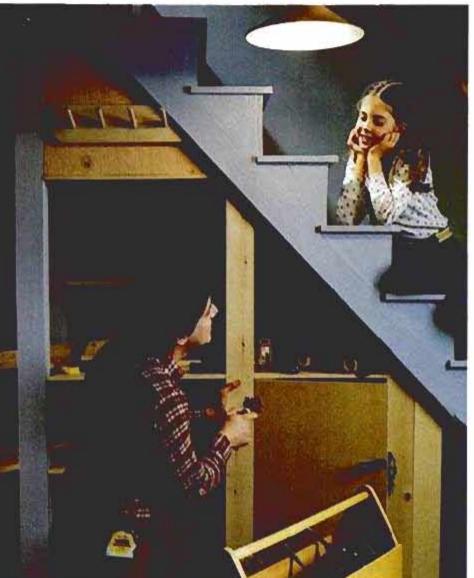
things going for it. It installs on both 11/4-in. and 11/2-in. pipe in just minutes, even if they are out of alignment. It can also withstand freezing without breaking, and if it becomes clogged, you remove the obstruction by squeezing it. From Flowtamer Inc., 666 Sugar Lane, Elyria, Ohio 44035, it's \$6.95 at home centers.-Rosario Capotosto

Home and shop update

It seems we got our chemical terminology mixed up in our story, Here's A Brush Cleaner that Works for All Paints, (page 168, Mar. '82). Methylene chloride is the compound used in paint cleaners and it is the one we intended to recommend. Chemists tell us that "meth"-compounds are numerous and some are very dangerous, but that methylene chloride is safe if used as we described. PM

If you've come across a new product with some special features, let us know about it. Write to PM Workbench, Popular Mechanics, 224 West 57th St., New York, N.Y. 10019.

YOU-CAN-DO-IT STORAGE: UPSTAIRS, DOWNSTAIRS, ALL AROUND THE HOUSE.



Stanley comes to the rescue! Just when you're absolutely sure you've run out of places to store things, Stanley comes up with a booklet crammed full of great ideas. Free, too. You can build (or build in) all these suggestions yourself—just as long as you're somewhat handy with tools and wood.

The list starts with a blanket chest/ toy chest. A behind-the-couch bookcase with hidden storage for valuables. A pantry on a door. Useful under-stairs storage, too. There are also some suggestions for a home entertainment wall, between studs shelving and much more.

To build these projects, all you need is wood and some standard hand tools—a Stanley saw, hammer, screwdriver, Powerlock* rule, to mention a few. You'll also want to pick up some basic Stanley hardware—screws, shelf brackets, nails, bolts, etc.

So go to it. There's a lot of satisfaction to be had in finding and building extra storage space around your house.

Order this free booklet from Stanley, The Do-It-Yourself Company that makes Stanley tools; hardware; weather stripping; shelf standards and brackets; fasteners; garage door openers; replacement entry doors.







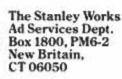
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DETROIT LISTENING POST

The large and small of it

Now that the vast middle ground is pretty well filled in with efficient new products, Detroit is looking hard at the fringes of the new car market—big, luxury cars, mini and microcars, and high performance sports and sporty cars. That's where most of the new product action will be in the next several years.

Honda recently introduced its City mini in Japan. Rumors have it that Mitsubishi, long the supplier of Chrysler's Japanese imports, may begin importing the front-wheeldrive Minica AMI for its own dealers as early as next year. So Detroit is scrambling for baby cars, too.

In Japan, the Mitsubishi AMI seats four, but weighs only 1,250 pounds and measures just 124 in. on a 79-in. wheelbase. Even in U.S.-legal form, it could get 50-plus mpg from its half-liter, two-cylinder engine. Chrysler may negotiate its own version of the AMI.

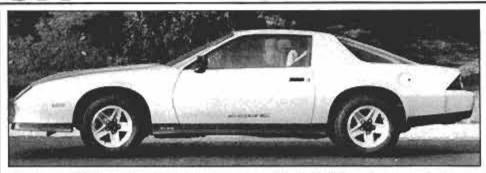
In 1985, Ford is looking into importing a similar-sized car from its Japanese partner, Toyo Kogyo, maker of Mazda cars and trucks and Ford's Courier mini-pickup. The Ford/Toyo Kogyo car also may offer an optional 50-plus mpg diesel.

Meanwhile, Chevrolet is firming up plans to import its own Japanesebuilt mini, beginning about the same time.

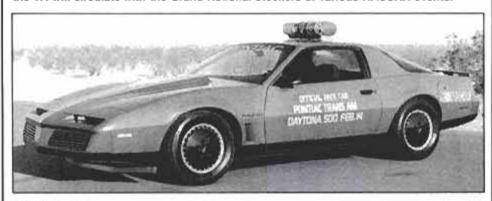
The vehicle will be styled by Giugiaro of Italy, jointly developed by GM affiliates Isuzu and Suzuki, and assembled by Isuzu. Isuzu already supplies Chevy's Luv mini-trucks and last year established itself as America's newest car and truck importer.

Slightly larger than GM's European S-car, but a foot shorter than the current rear-drive T-car (Chevy Chevette/Pontiac T1000), the diminutive GM/Isuzu/Suzuki four-seater should measure about 150 in. overall on a 90-in. wheelbase, weigh around 1,600 pounds and deliver over 40 mpg in the city. It will be powered by an overhead-cam, four-cylinder engine displacing 1.0 to 1.3 liters and will come with a standard, fivespeed, manual transaxle.

With fuel supplies high and prices actually slipping at the moment, short-term U.S. demand for such vehicles is not judged to be very high. But it looks like Detroit has decided to prepare itself for changes in the situation.



The Camaro Z28 (above) and Pontiac Trans Am will both be fighting for your attention on the racetracks this season. The Chevy is the official pace car for the Indy 500 May 30, while the TA will circulate with the Grand National stockers at various NASCAR events.



Full-size futures

At the other end of the market, GM's '84 front-wheel-drive C-cars—Olds 98/Buick Electra/Cadillac deVille and the Brougham—seem to be progressing on schedule. Riding on a 108-in. wheelbase, these will be about 2 feet shorter and a half-ton lighter than their current counterparts and are expected to retail for close to \$20,000.

Ford plans to come in a year later with new fwd, 106-in.-wheelbase LTD/Marquis replacements. Codenamed Taurus, these will be four-cylinder, five-speed-powered in their standard form, and about the size of today's Granada. A year or so later, an all-new breed of Lincolns will bow on the Taurus platform.

The new muscle car era

Most everyone in Detroit has gotten the message by now: The market is getting bored with economy and ready for renewed excitement—cars that stir the libido, that people buy because they want to, not because they have to. The rebirth of convertibles is one result, and renewed interest in performance is another.

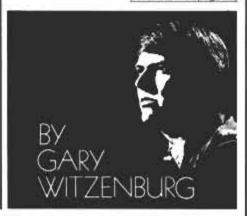
Chrysler engineers are working on supercharging small engines and turbocharging larger ones for extra power at low speeds. Pontiac is investigating a turbo version of its

fuel-injected, 1.8-liter overhead-cam Four for future J2000s, and Buick is developing a turbocharged 3.8-liter V6 with multipoint fuel injection for coming sporty models.

Ford, which is back into racing and marketing excitement for all it's worth, has its Special Vehicle Operations working up super Mustangs and Capris for '83 that'll be "the best-handling cars Ford Motor Co. can do." They will have turbo four-cylinder power and perhaps an optional turbo Six, as well. And Chrysler is on schedule with a Porschelooking fwd sporty car with optional turbocharging for the '84 model year.

On the convertible front, Chevy has confirmed that its ragtop Cava-

(Please turn to page 52)



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DETROIT LISTENING POST

(Continued from page 50)

lier J-car will go into limited production for '83. American Sunroof will do the conversions. Ford is already thinking of moving its soon-to-be introduced soft-top Mustang by Cars and Concepts in-house, if demand justifies it. Pontiac displayed a J2000 convertible at the Miami Auto Show last December, and says it may or may not be produced. But the open-air Pontiac was at last February's SAE engineering show.

Automakers should take note,

however, of a recent J.D. Power & Associates automotive consumer profile survey. It showed that, while fuel economy had slipped to third on the list of what buyers want from their new cars, dependability/trouble-free operation, and low purchase price were first and second.

Pontiac pace car

Chevrolet's Camaro Z28 is the Indy pace car for '82 (Imports and Motorsports, page 20, May '82). But archrival Pontiac has the NASCAR safety car assignment for the 11th straight year. Watch for the bright red Firebird Trans Ams at Daytona, Talladega, Charlotte, Darlington and other famous NASCAR tracks. One reason, says Pontiac, is that with its drag coefficient of .309, the new Trans Am just may be the most aerodynamic production car in the world today.

Ford's going Aero

Last month, we talked about the Continental Concept 90, the striking new Lincoln-Mercury idea car that's traveling the auto show circuit. Now Ford has confirmed that this new car provides an excellent preview of its products.

It's no secret that this Lincoln of the future—except for details like the grille, wheel covers and pillarless hardtop roofline—looks a lot like the sexy '83 Thunderbird due for introduction in January 1983. Privately, Ford designers say they think everything else will look old fashioned once their slippery new

cars start hitting the road.

The truth about diesels

Partly because of their well-known fuel efficiency and range, and partly because they've attained a certain amount of status appeal, diesel-powered cars have enjoyed great sales success in the past few years. But there's talk around Detroit that perhaps diesel growth has just about topped out for the moment.

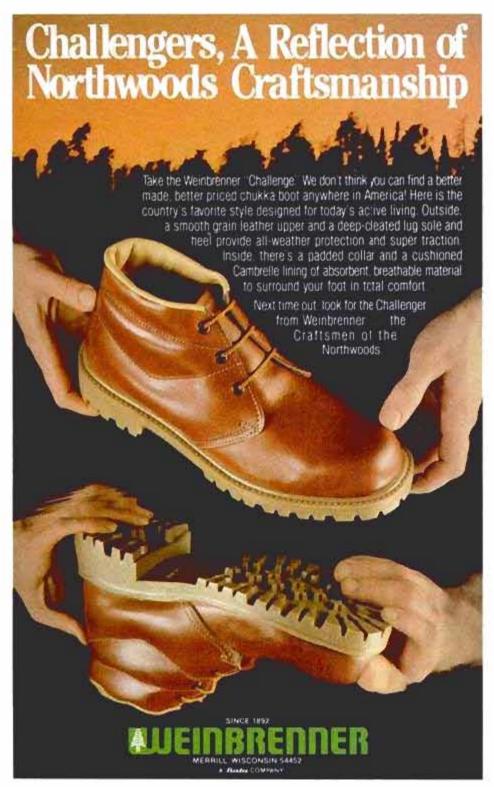
Now Stanadyne Diesel Systems, America's leading maker of diesel fuel injection equipment, is speaking out in an understandable effort to win more converts. Their pitch:

Diesels are long-lived and, because they're compression-ignited, don't need the regular electrical tune-ups required by gas engines. They do need more frequent oil changes, though.

Diesels are more expensive initially by about \$200 to \$1,000, but offset that cost through fuel and maintenance savings. Of course, the amount depends on how long you keep the car and how much you drive.

Stanadyne says that the diesel's disadvantages—sluggish acceleration, difficult cold-weather starting, water-in-fuel problems—can be taken care of by, respectively, turbocharging, electric engine block and fuel heaters, and warning systems, water separators and filters. Stanadyne, in fact, recently introduced an easily installed aftermarket fuel heater.

Manufacturers have made good progress in reducing other minor diesel annoyances such as noise, smoke and odor, and Stanadyne predicts that diesel acceptance will continue to increase in the future.



If your old guzzler is on this list, you're in luck.



8 Cyl.—1968-76 304", 360" 2 bbl. engines, all transmissions. 6 Cyl.—1970-76 232", 258" 1 bbl. engines, all transmissions.

BUICK

8 Cyl.-1970-76 350"-455" 2 & 4 bbl. engines, all transmissions. 6 Cyl.—1968-75 250" 1 bbl. engines, all transmissions, 1978 231" 2 bbl. engines, all transmissions.

CHEVROLET

8 Cyl.-1968-76 327"-454" 2 & 4 bbl. engines, all transmissions. 6 Cyl.—1963-74 230", 250" 1 bbl. engines, all transmissions. 4 Cyl.—1973-77 140" 2 bbl. engines, all transmissions.

DODGE/CHRYSLER/PLYMOUTH

8 Cyl.—1970-77 518"-440" 2 & 4 bbl. engines. all transmissions. 6 Cyl.—1969-77 170"-225" 1 & 2 bbl. engines, all transmissions. 4 Cyl.—1979-80 1.7 litre 2 bbl. engines, all transmissions.

FORD/MERCURY

8 Cyl.—1962-77 289"-400" 2 bbl. engines, all transmissions. 6 Cyl.—1965-77 170", 200" (Incl. 2800cc) 1 & 2 bbl. engines, all transmissions. 4 Cyl.—1971-74 1977 2000cc, 2500cc 2 bbl. engines, all transmissions.

OLDSMOBILE

8 Cyl.—1975-77 350"-455" 2 & 4 bbl. engines, all transmissions. 6 Cyl.—1968-78 231", 250" 1 & 2 bbl. engines, all transmissions. 4 Cyl.-1976-77 140" 2 bbl. engines, all transmissions.

PONTIAC

8 Cyl.-1967-78 305"-455" 2 & 4 bbl. engines, all transmissions. 6 Cyl.—1970-78 251", 250" 1 & 2 bbl. engines, all transmissions. 4 Cyl.—1975-77 140", 151" 2 bbl. engines, all transmissions.

TOYOTA

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Economy carburetor that can improve your car's gas mileage from 5 to 10 percent* depending on engine. How's that for luck?

The ECONOMASTER brand Fuel Economy carburetor is the only replacement carburetor designed to deliver improved fuel economy. Many models have enlarged metering ports in the carburetor's idle system to improve fuel conditioning. And an exclusive vane-style booster venturi for better fuel atomization. Plus twostage power valves for improved mid-range efficiency. So you can put that old guzzler on a diet. Fast. If your car isn't on this list, you may still be in luck. There are ECONOMASTER brand Standard

Replacement carburetors for many cars. It can restore your car's original performance. Holley Replacement Parts Division; 11955 East Nine Mile Road.

Warren, Michigan 48090.

*Actual economy may vary due to application, engine and driving conditions, engine modifications and driving habits. For passenger cars only. *Registered Trademark

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It's a quality-crafted, polished aluminum beauty ready and waiting for 12 frosty ounces of your favorite brew or soft drink.

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And get one for the road. Free.



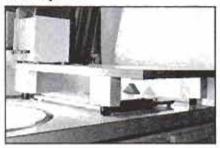


Our reputation rides on everything we make: 19007 S. Reyys Ave., Compton, CA 90221.

TURBO POWER At city speeds. exhaust gases travel ir economical reverse-flow pattern. At highway speeds, gases automatically bypass battles, cut backpressure for maximum fuel efficienty the fuel-efficient muffler!

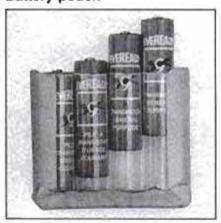
PHOTO HINTS

Free space



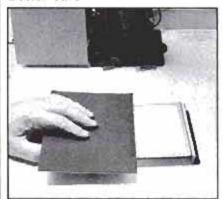
Keep your enlarger off your darkroom table with four scrap pieces of 2×4, each about 3 inches long. This will leave space for a print easel and hundreds of sheets of paper.—Robert Hertzberg

Battery pouch



Remove dry cells from equipment when not in use to prolong their life. Store them in bubble material they came in. Cut bubble from cardboard and tape its two sides to form pouch.—A. Weber

Better safe



An overly bright safelight can fog prints. Cover half a fresh sheet of paper in easel with a black card for two minutes; develop it. If uncovered half is darker, light is too bright.—Robert Hertzberg

NEW NOW for home and shop





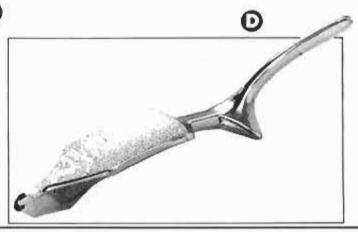


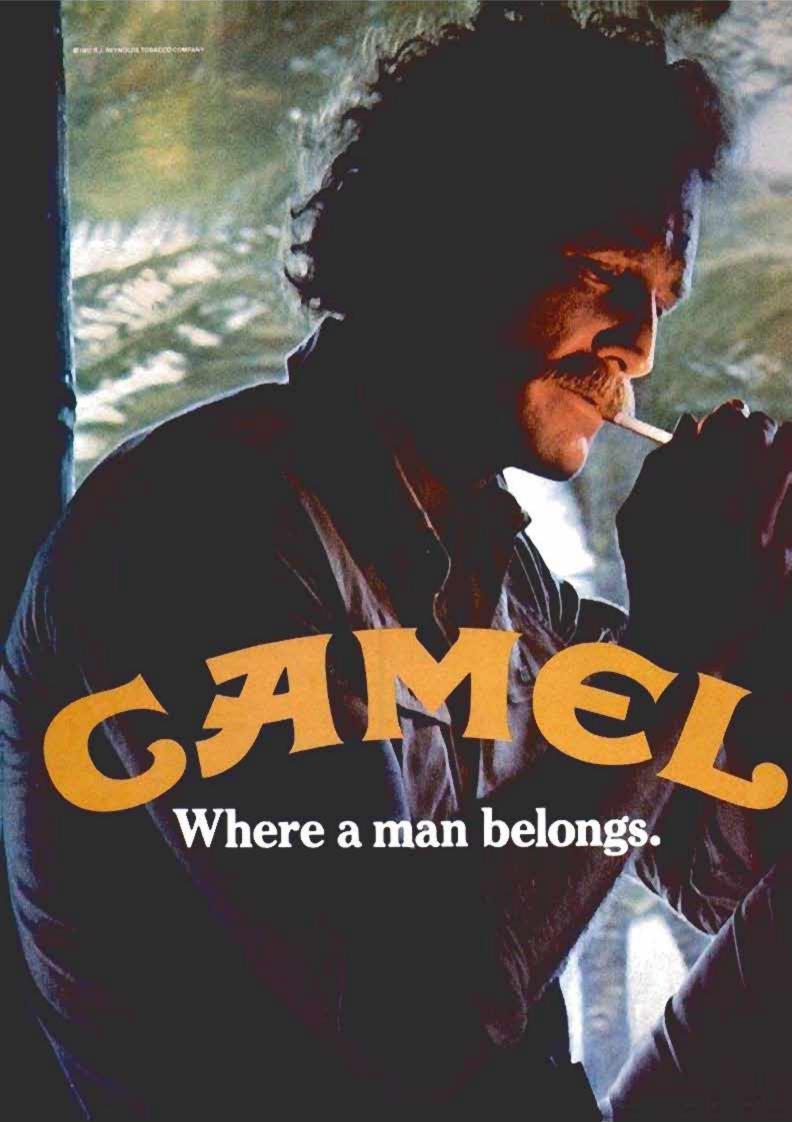
A The Miser is a new 120-watt reflector lamp designed to produce the same usable light as a 150-watt spot or flood lamp. According to the manufacturer, a new reflector with fluted sides directs the light more accurately, thus providing the same illumination with a 30-watt savings. The new lamps are interchangeable with all medium-screw-base 150-watt lamps and can be used both indoors and out. Made by General Electric Co., Nela Park, Cleveland, Ohio 44112, they cost about \$6 apiece at hardware stores and home centers.

B Heirloom is a new style of replacement plumbing fixtures from Dolly Trim Inc., 3222 East Willow St., Signal Hill, Calif. 90806. The wash basin unit shown features washerless valves, ¼-turn nonrising stems and metal or porcelain handles. A matching bath fixture (not shown) has an adjustable valve stem that compensates for differing valve depths; this allows you to replace most old shower fixtures without having to tear apart the shower wall. Both fixtures are made of solid polished brass and cost about \$280. If they are not available at your local plumbing supply store, write to the manufacturer.

C Pine Crest 2 is Champion's newest style of knotty pine lumber paneling. It comes in ¾ × 3½ × 96-in, reversible tongue-and-groove boards. One side is rough sawn, while the other is smooth, with beaded grooves cut so the surface simulates two 2-in.-wide boards (shown). It can be installed vertically—with the use of blocking between the studs—and diagonally and horizontally by simply nailing boards to the studs. It's from Champion International Corp., 1 Champion Paza, Stamford, Conn. 06921. A 10-board package (about 24-sq.-ft, coverage) sells for around \$21 at home centers.

The MasterCutter is a new, all-purpose glass cutter with a nonslip finish made to fit the contour of the user's hand. According to the manufacturer, this design provides better control and less wrist fatigue over extended periods of use. Aso featured is a replaceable carbide steel wheel. It can be changed for one of five other wheels that are designed specifically for the type of glass being cut. The tool lists for \$12.99 at hardware stores and home centers. It's from Red Devil Inc., 2400 Vauxhall Rd., Union, N.J. 07083.









New Carburetor And Manifold Yield Better MPG

The Z System certainly works. But your payback period may take too long.



Holley Z Manifold has a divided plenum chamber for better fuel distribution and vacuum signal at low engine revolutions.



The 450-cu.-ft.-per-minute (c.f.m.) carburetor is matched to the manifold's characteristics. All the emissions gear is retained.



The free-flow, paper, air-cleaner element and housing is part of the kit. It lets the engine breathe more freely than stock unit.

You must have noticed the ad: Zora Arkus-Duntov, father of the Corvette, looking tan and fit, is standing next to a Camaro Z28 surrounded by oversize engine parts. The headline reads, "Designed for new levels of efficient performance," and the copy idles on about "optimum performance combined with fuel efficiency and driveability."

Despite appearances, Zora isn't selling Chevrolets, but rather a new intake manifold/carburetor combination. Holley calls it the Z System. And Zora Arkus-Duntov is more than Holley's answer to Ricardo Montalban. Not only is Duntov the celebrity "spokesman" for the Z System, but he designed it and holds the patent.

According to Holley test and development engineer Frank Walter, who worked with Duntov, almost every carbureted engine has a fuel-feed problem; the cylinders farthest from the carburetor tend to run lean. In order to compensate, you have to run a too-rich mixture to the cylinders near the carburetor. The result is wasted fuel, loss of power and a hesitation under low-rpm acceleration.

When Zora Duntov was hired as a consultant, says Walter, he brought with him a manifold design to eliminate this uneven distribution. Duntov's idea was simple, at least in theory. He thought that he could boost the signal in a given manifold runner by the use of a resonating chamber. "He had this all mathematically plotted out," says Walter. "But as is often the case, the theory didn't work on an actual car.

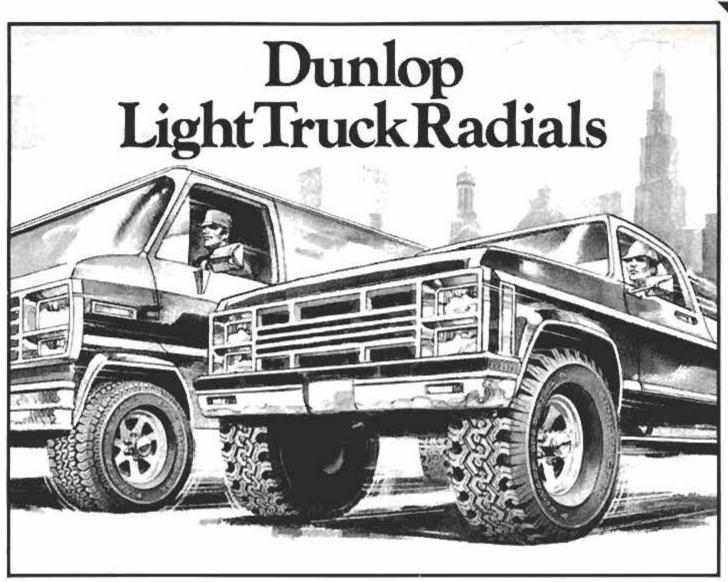
Theory and practice

"What we ended up doing was trial and error development, trying different things: little pocket valves, resonating chambers on all the cylinders, interconnecting the cylinders. We were using the small-block Chevy V8 for development, and found out the most significant improvement was when we tied cylinders seven and eight together.

"There was a lean mixture in these cylinders, and cylinder five tended to scavenge from cylinder seven. By connecting cylinders seven and eight together with a resonating chamber in the manifold, we were able to boost the fuel charge in cylinder seven, and to a lesser extent, in eight. This increases the efficiency of the fuel distribution in the manifold, which in turn allows you to lean out the carburetor for lower emissions, better fuel economy and, usually, better driveability.

"Then we added a divider in the plenum chamber under the carburetor. This isolates one side of the carburetor from the other. The 'open plenum' manifold without a divider is good for breathing at high rpm, but it isn't as sensitive to throttle variations at low

(Please turn to page 69)



Durable, Dependable and Economical.

If you carry your livelihood in the back of your truck, you need tires that work as hard as you do. Dunlop light truck radials. Day in, day out they deliver cost saving benefits and dependable performance ... but that's what you'd expect from one of the pioneers of light truck radial tires in America.

Dunlop radials stretch your tire buying dollars with longer tread life than bias ply tires. And, the lower rolling resistance of radial construction means fewer stops at the gas pump.

Out on the job, Dunlop light truck radials keep working from sun-up to sun-down with twin steel belts that protect against road hazards; sidewalls that resist cuts, bruises and abrasions; and dual polyester body plies that absorb road shocks. Job after job they deliver comfort, control and easy handling.

There's the Gold Seal LT Rib... an all position highway radial with a wide five-rib tread pattern.

Or the Gold Seal LT Traction . . . a rough road, bad weather, mean condition workhorse with a wide, deep traction tread for on and off

road service.

The Gold Seal LT Rib and Gold Seal LT Traction . . . use them in sets of four, or in tandem with the LT Rib on the front and the LT Traction on the rear. Either way they'll work as hard as you do.

Dunlop light truck radials. Built tough. Built smart. Built for the way you work.



LT RIB

LT TRACTION



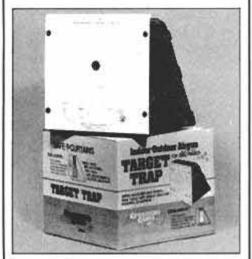
ALL OUTDOORS

Better bull's-eyes

Air guns, particularly the new adult models, can supply great accuracy, are quieter than firearms, and can be used for target practice indoors as well as out. With care, a basement or back yard can become a target range. The newest aid to make this changeover easy is a Target Trap Model 850 from Crosman Air Guns. It's true that after studying one in a store you may be tempted to make your own. Behind the bull's-eye are three curtains of material that dampen the impact of a BB or pellet the way a blanket on a clothesline acts as a good backstop for baseball pitching practice. Behind the trap's curtains is a slanted metal plate to deflect and collect the BBs.

But at a price of only \$17, the Crosman model is probably better than a homemade one. A phone book, cardboard box or plywood plank—some of the other favorite backstops—have been known to bounce pellets back at the shooter or around the basement. Even the frame edges of the Crosman target trap have been rounded and slanted to prevent this. The added safety is worth the modest expense.

Equally inexpensive are a set of scaled-down metal silhouettes—the standard turkey, chicken, pig and ram—designed just for air-gun practice and \$5 at Crosman dealers. Beeman Precision Airguns, San Rafael Calif., and their dealers, have printed silhouette targets scaled for a 10-meter (33-foot) air-rifle range. There are five targets of each animal mentioned above for a total of 20 trys. And there's enough border on



Designed for Indoor or outdoor air-gunning, new Model 850 Target Trap by Crosman (\$17) is for year-round practice.

the targets so you can see where the shots are going if you miss. There's also a new short, rubber-coated 2.5X-power telescopic sight for air rifles and rimfires for under \$100 from Beeman.

Car camping

Turning your auto into a recreational vehicle is still one of the bargain ways to travel, and new small tents, sleeping bags and cooking gear all



From cool drinks and food to maps and trip notes, handy storage is easy in an Igloo Kool Rest ice chest for auto travel.

help cut costs for a family vacation trip. The Kool Rest, a car-seat ice chest from Igloo, in Houston, can sit between front- or rear-seat passengers, be strapped in with a safety belt, and hold two six-packs or sandwiches, maps, toll coins and dark glasses. Igloo also has a new midsize, 34-quart ice chest especially designed to fit the luggage compartments of new compact cars.

Places to camp are listed in national directories, like the \$1 KOA Handbook & Directory, Box 30558, Billings, Mont. 59114, and various state guides. The Colorado Campground Assn., 5101 Penn. Ave., Boulder, Colo. 80303, has one for \$1 by mail. And a Family Camping Vehicles Publications Catalog lists booklets such as RV Repair and Maintenance, Energy-Saving RV Tips, and Living in Style-The RV Way and 33 others. All are available from RVIA, Box 204, 14650 Lee Rd., Chantilly, Va. 22021, if you send a business-size, self-addressed, stamped envelope.

Cool comfort

This month, warmer weather returns to the northlands of the country, but not yet to their waters. Charlie Walbridge of the American Canoe Assn., Lorton, Va., suggests that any time water and air temper-

atures add up to less than 100°F., a wetsuit should be worn under your life vest. Nearly 50 percent of all canoeing accidents occur early in the season, and anyone working near water still chilly from winter runoff should take precautions to prevent hypothermia, especially if there is a chance of falling in. The ACA Safety Committee recommends refresher canoeing instruction at the beginning of the season for paddlers with limited experience, plus a personal flotation device that is worn at all times.

Folding firearm

For camping and survival shooting, a new over-and-under has been announced by the Springfield Armory, Geneseo, Ill. This one folds up conveniently, fires a single-shot .22 long rifle in the top barrel, a 410-bore shotgun shell in the bottom, and stores nine additional .22 cartridges plus four 410 shells in the stock—all at a weight of under four pounds.

It's a modernized version of the World War II Air Force M6, has a mil-

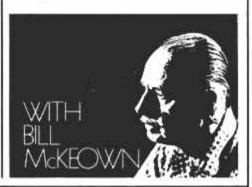


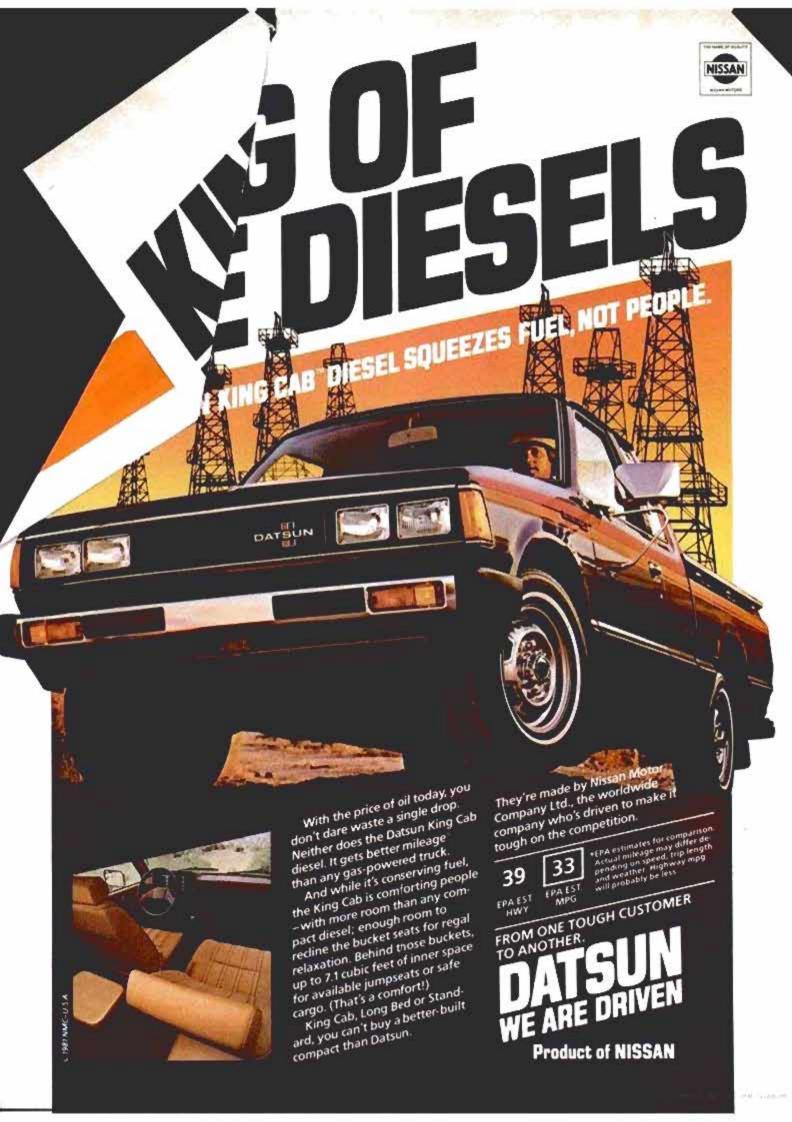
The new M6 Survival Gun from Springfield Armory unfolds to fire a .22 bullet from top barrel and a 410 shot shell from bottom.

itary Parkerized finish and can fire 410 signal flares, as well. Base price is about \$140.

Backpack beast

For hikers who like to share the burden, Early Winters Ltd. in Seattle is offering trained and friendly llamas for up to \$2,000 f.o.b.





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house construction, including how to read plans and make your own..

how to lay out a home workshop . . . and more.
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You get a complete how-to book on every aspect of roofing, flooring, doors,

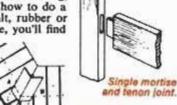
You get a complete how-to book on every aspect of roofing, flooring, doors, windows, stairs, plus how to use stationary and portable power tools, plus tips on painting, maintenance, repairs, termite protection... and more. You get step-by-step instructions on everything from reading a building plan to building a house (including the foundation), doing all the carpentry in it—and keeping the house in A-1 shape for a lifetime. And thousands of show-how pictures make it all amazingly easy, even for a novice. Let's look at a few of the remodeling and fix-it jobs you learn how to do ... You learn how to replace worn or missing shingles. How to fix chimney flashing leaks. How to repair stairs (or build them from scratch).

If doors need fixing or fitting, you can find out how to

If doors need fixing or fitting, you can find out how to do it from these books. If you want to lay new flooring, you can just turn to these books to find out how to do a first-rate job with wood-tile, linoleum, asphalt, rubber or ceramic tiles. If you want to build a fireplace, you'll find out exactly how from these books...

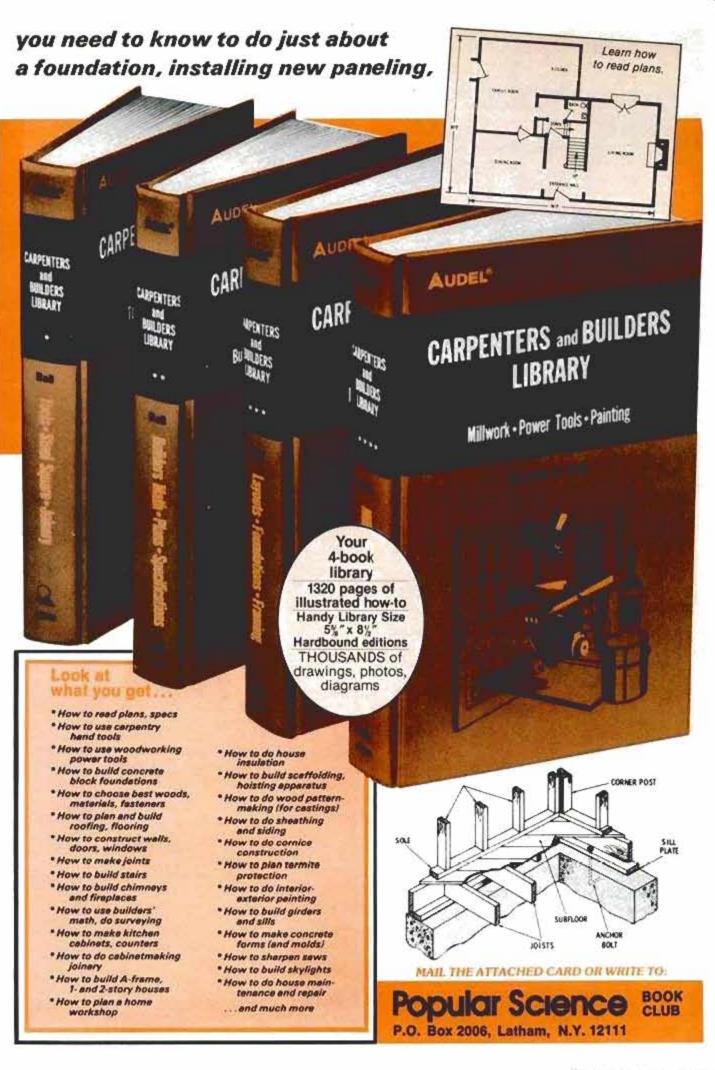
This marvelous 4-book library also tells you how to use all kinds of power

tools to maximum advantage (most of them have great versatility). You learn about woods—their character-istics, strengths, best uses. You learn about fasteners-nails, screws, bolts, more-their holding power, size-



installing metal shingles under wood shingles.

strength ratio, and how to use each. These books teach you scores of tricks and shortcuts used by carpenters for measuring cutting, raising, fastening, finishing-every technique and job. They give you the answers to just about every carpentry question you'll ever have. And they can save you thousands of dollars in the years ahead.





Hug the road better...longer.

Bobby Unser, auto expert and champion driver says "With Gas-a-just Shock Absorbers you'll hug the road better than with ordinary shocks because Gas-ajust's patented design seals in nitrogen gas and all-weather fluid under pressure to give you smoother, consistent performance throughout the full stroke of the shock absorber."





Gas-a-just Shock Absorbers And this superior performance lasts throughout your trip because Gas-a-just's design eliminates the foaming and aeration which is so common in ordinary shocks and causes their performance to become erratic and fade as you drive.

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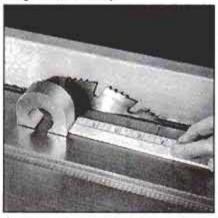
HINTS FROM READERS

Salvaging hacksaw blades

Although I usually buy highquality tools, when it comes to hacksaw blades, I find that the least expensive ones are the best buy. I try to get a 20-blade pack for about \$1.50. The blades I buy are extremely hard; on the other hand, they also break easily.

These blades are still a good purchase, however, because I save and reuse all the broken pieces. I grind some of them to fit my sabre saw. Recently I cut a 40-gallon hot-water heater in half lengthwise, using some oil and three broken blades. I use other fragments as scrapers; many scraper shapes are formed when the blades break. Sometimes, I grind them to suit a certain job. Last summer, I reglazed and painted all the windows in my house with the help of these broken blades.—John Nelson

Magnet saw stop



A strong permanent magnet can serve as an improvised and easily adjustable stop for a circular, band or jigsaw that has a metal table. The stop simply limits the length the workpiece is cut. Toward the end of a cut, feed the work slowly so the magnet won't shift. The photo shows an Alnico magnet used as a table-saw stop. The magnet is a blend of aluminum, nickel, iron and cobalt.

In setup above, the rip fence is positioned close to the saw blade. A stop clamped to the fence would be impractical. When the first series of cuts is completed, adjust the magnet to make the next cut. Alnico and other magnets are available by mail from Edmund Scientific, 101 Gloucester Pike, Barrington, N.J. 08007; catalog, \$1.

-Walter E. Burton

ANATOMY OF A WING-WALK

(Continued from page 31)

approached the record slowly and carefully. We made tests. For example, regular nylon rope isn't any good because it stretches. We hunted all over for special rope until we found Larry Caldwell of Pigeon Mountain Industries in Georgia. He had a rope with only 2 percent stretch and an 11,000-pound test strength.

PM What about the right wing?

the right side. We planned it so Steve Weiss would jam his body against the door, plant a foot on the wing, and block the wind for the rest of the guys. But when he did, the airplane shuddered and shook like crazy. Things ripped off the cabin walls; we lost headsets and mikes—they just zapped out of sight. We gave the emergency signal and everybody jumped for their lives.

PM What went wrong?

MC Like I said, this was an engineering adventure. When Weiss put



I checked with a whole bunch of former Ju-52 pilots from Germany and drew a blank everywhere. I went over the possibilities for this and many other things, by the way, with Dr. John Hoover of the University of Florida. He did engineering, air drag and other tests for us. The only way to find out was to try it. So we removed the door, crossed our fingers and took off. We got a strong burble going through 90 mph on the climbout and I did a whole series of maneuvers in the air, going up and coming down. Any time we went through 90 mph, the burble shook the plane, then quit, Great! So we got ready to make a test with six guys on the left wing and six more on the right.

PM And how did that go?

MC It scared the hell out of us. We put Dave Kanamine on the left wing with a bunch of guys, and the Ju-52 was shaking and kicking up a fuss and then six more guys started out

his foot onto the wing, it was at the leading edge and close to the root: the worst possible place. The air ripped around his body, in the door and smashed right through the airplane.

Actually, it was a good thing, because it made us think. If you kill the lift between the outside arc of your prop tips and the fuselage on just one side of this type of plane, you're destroying 40 percent of all your lift.

PM So the decision to use only one wing was made for you.

MC Definitely. It was safer because we'd have more lift and more control.

PM You said you were going for 18 people, but you set a record with 19. How come?

MC Major Don Towner of the U.S. Air Force joined up with us. He took

(Please turn to page 68)



SMALL ENGINES WORK HARDER THAN LARGER ONES

Yes, 4 cylinder engines work harder than 8 cylinder engines to travel the same distance. When the 4 and 8 cylinder engines have both traveled 10,000 miles, the 4 cylinder engine has worked the equivalent of nearly 20,000 miles. They also run hotter — tending to shorten the life of your oil by creating varnish, our and studge.

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We had 24 people aboard: I

had left seat, Don Yahrling was in

the right seat, Bill Tharp flew as

crew chief, Art Barchie was jump-

master and Mike Fuller had a cam-

era, plus the 19 wing-walkers. Out on

the left wing was a big vertical boom

with a couple of cameras, and Ken

Lindsley and Gene Chevalier had

stuck cameras all over the plane. We

were at 23,000 pounds and had a hell

of a lot of drag. We thought that we'd

need up to 4,000 feet for the takeoff

roll, and my first surprise came

when we used 25° of flaps for takeoff

and jumped into the air after just

1,200 feet. We flewover Palatka, Fla.,

our designated drop zone, with three

chase and camera aircraft. We had

planned to go at 9,500 feet. But peo-

ple moving around the Ju-52 got me

into a bad center of gravity position

point position behind the left engine

and Mike Maguire held lead along

the fuselage. They blocked the wind

OR YOUR MONEY BACK.

ANATOMY OF A WING-WALK (Continued from page 67)

some fabulous films to document the event and he made it 19.

PM Did you run into any other unexpected problems?

Every time we flew a test. For example, we made our early tests with up to 12 people on a wing. We figured we would need every ounce of lift, so we used 15° of flaps. Big mistake. All of a sudden, the airplane put its nose down sharply and dived almost vertically, out of control. We dumped the crowd and recovered.

PM What was wrong?

When we put the people on the wing, we added camber, or curve, to the upper surface of the wing. This increases lift and does a couple of other things. The flaps at 15° further cambered the wing. Presto! The center of pressure under the wing moved aft and the center of lift moved forward. The turbulence off the bodies on the wing unloaded the tail surfaces, which went up, and the nose went down, and all together, wow! We became a finned bomb. After that, we changed positions on the wing and left the flaps up.

Tell us about the parameters you finally selected for the record wing-walk.

and I started the final run at 9,000 feet, letting the speed build up so that at 8,500 feet, we were indicating 120 mph. PM So they started onto the wing at 8,500 feet? Yes. About six of them went out fast. Dave Kanamine took a

for everybody else. My first real big surprise in the air was a sudden. severe yaw of the nose to the left. I kicked right rudder as hard as I could and she came around. Our speed went to 140 mph indicated. faster than I believed, and she put her nose down as that gang kept pouring out on the wing. And just like that, she went through a 2,000, then a 3,000, then a 4,000 feet-perminute rate of descent.

Did you lose control?

For a while, we didn't think we could hang on to her. The buffeting was wild, really bad. We got a pogo effect, up and down. The left wing was twisting wildly-you could see it flexing like mad-and the right wing was drumming like a washboard. Then we heard this terrific booming sound. She was oilcanning. Our speed built up to a true 190 mph and the yaw was terrific. She was trying to roll over on her back. By now, I was holding full left rudder and full right aileron, so there we were, coming out of the sky with full crossed controls. I didn't think the airplane was going to stay together.

The voke columns were shaking as if they were going to bust right out of the floorboards and I couldn't read the instruments anymore because of the vibration. I had to kill power because we were accelerating so fast. I almost hit the smoke switches for the emergency signal to bail out. Tharp was screaming that we had only four more to go. I thought he meant four more people, but it was four more seconds for the prearranged eight-second ride. I glanced at the left wing and it was twisting and rippling. Then I heard Tharp yelling on the radio, "They're going!" He meant the jumpers, not the wings. As the wing-walkers were spilling off, things were smoothing out and, suddenly, we were back in the real world with a docile airplane. coming downstairs like a bat out of hell, but smooth. And we'd done it! Did the airplane suffer any

damage?

MC We busted a couple of things, but they were minor. We were amazed at that. One chase plane pilot said the whole right wing was a blur of vibration and the tail was twisting in a crazy whirl mode. He didn't know how it was staying on.

PM What's next on your agenda of nonstunts?

MC A new world's wing-walking record.

PM You're kidding.

MC I'm serious. All women: 21 or more.

PM Bikini-clad, no doubt.

MC You've got it.

THE HIGH-MILEAGE FILES

(Continued from page 58)

rpm. That big plenum acts like a large mixing bowl. By splitting the plenum, we can increase the flow rate in the manifold runners to the cylinders. That's why you get better throttle response at the bottom end.

"We put this manifold together with our small, four-barrel carburetor. Now, most people think that when you go from a two-barrel to a four-barrel, your gas mileage will go down. But at highway speeds, you're driving on just the primaries of the four-barrel, which are smaller than the two-barrel bores. We pick up economy on most two-barrel to four-barrel conversions. It's perfectly logical, when you think about it."

Trying out the Z System

It all sounds perfectly logical, but does it work? We decided to put Duntoy to the test.

A complete Holley Z System starts with Duntov's patented manifold, cast in aluminum, Model 300-28Z. With it comes a matching Holley Model 4360 four-barrel carburetor rated at 450 cu. ft. per minute (c.f.m.). Holley also includes a 14-inch, "free-flow" air cleaner and a high-output fuel pump, rated at 80 gallons per hour at 4.5 p.s.i. Finally, there's a pair of polished aluminum valve covers. They don't do a thing for performance, but they make your engine prettier.

All of this is not cheap. Holley's suggested retail price for the complete package is \$684.60, though almost any auto parts store will discount this price to around \$360, which still isn't peanuts.

The Z System is available for a variety of engines, but we used a small-block Chevy similar to that for which it was originally developed. Ours is a 305-cu.-in. version, fitted in a 1979 Nova.

At a steady 60 mph on the highway, the 3,000-plus pounds of the Nova required a gallon of unleaded premium every 22.50 miles with the stock two-barrel carburetor. With the Z System, fuel economy improved to 23.84 mpg, a solid gain of 1.34 mpg or 6 percent. Exhaust emissions were reduced, too: HC dropped from 380 parts per million (ppm) to 36 ppm, CO from .22 percent to .03 percent. CO₂, however, went from 12 percent to 14.9 percent, indicating more efficient combustion and cleaner burning.

But that's not the half of it. Since the day it was born, this Nova has suffered from annoying detonation, asthmatic acceleration and a general overwhelming feeling of lethargy.

The Z System changed its life. The detonation is gone, at least on 91-octane gas. Acceleration is noticeably snappier. And best of all, for the first time, the car responds when you hit the gas.

According to Holley engineer Frank Walter, this is just the beginning of improvement. An aftermarket camshaft, headers and exhaust system will pull the cork from the exhaust side of the V8, an absolute necessity now that we've opened up the intake side.

The bottom line is cost. Is it worth

replacing your stock intake system with \$360 worth of Zora Duntov's scheming? Well, if you drive 10,000 miles a year and pay \$1.40 for a gallon of gas, you'll save a piddling \$35 a year. So your payback period would be just over 10 years. In other words, it's not worth the trouble if all you're after is saving gas.

On the other hand, our test Nova transformed was just as reliable and easy-starting, but much more enjoyable to drive.

> Produced by the PM Auto Dept. Text: Rich Taylor



THE LEADER OF THE PACK.

Ford wagons. America's bestselling wagon lineup. Again.

Ford, the Wagonmaster, has been pleasing wagon owners for over 50 years. In fact, since 1949, a million more people have bought Ford Wagons than any others.* LTD Country Squire. A fine car and a wagon, too.

LTD Country Squire, a
Wagonmaster tradition, still
offers full-size room and quiet
comfort. There's room for
eight passengers with the
Dual Facing Rear Seats option.
And LTD has more cargo room
than any wagon in America.**

Almost 90 cubic feet with the rear seat down.

Ford Granada. The first Granada wagon ever.

Ford is proud to introduce the first Granada wagon with all the style of Granada and more cargo room than any American-built wagon in its



FORD

class." Granada wagon for 1982, built with Ford's commitment to quality.

Ford Escort. The bestselling wagon in America.

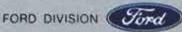
Escort is the only American-built wagon with both the traction of front-wheel drive and the smoothness of four-wheel independent suspension. And Escort was designed

with special high-strength steel to achieve lighter weight and excellent fuel economy.

For comparison. Your mileage may differ depending on speed, distance and weather. Actual highway mileage lower.

Whether you buy or lease, see the LTD, Granada and Escort wagons at your Ford Dealer now.

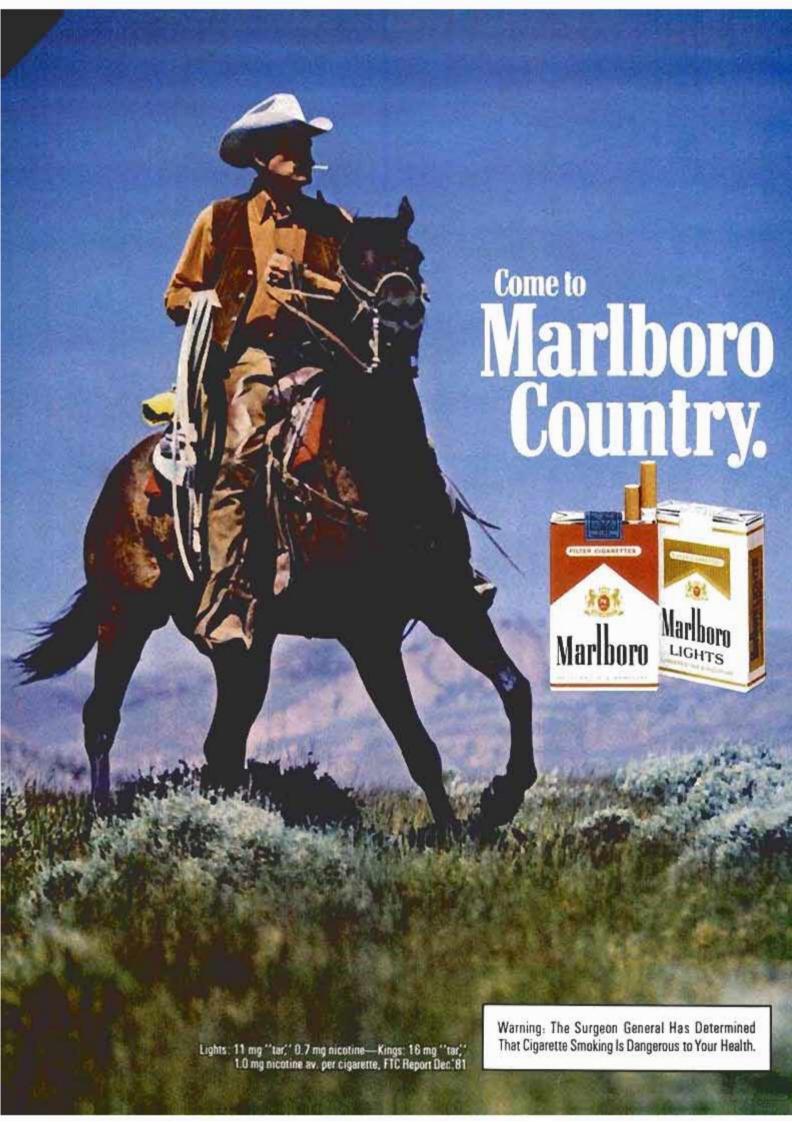
FORD WAGONS





WAGONS

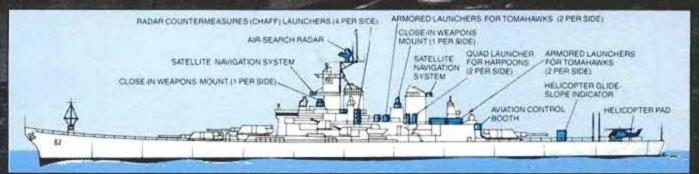




Popular Mechanics

Born-Again Battlewagons

New Jersey (photo) before 1969 decommissioning. Drawing below shows reactivation profile with aviation area, plus new weapons.



Critics call them tempting targets, but the Navy says the updated mix of missiles and BB guns is worth a lot more than four on the shore.

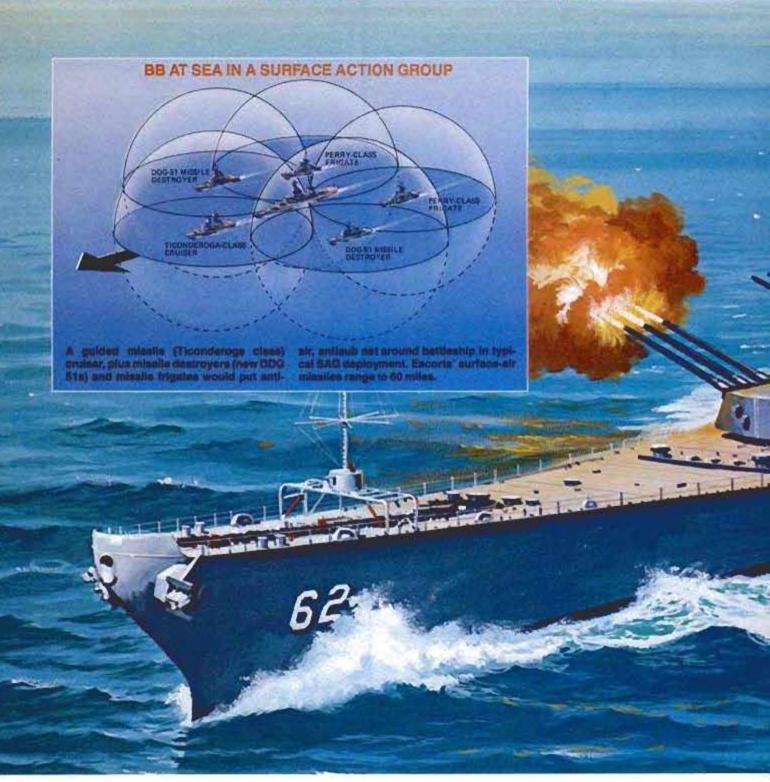
by Robin Nelson TECHNOLOGY EDITOR

It didn't take long: Only a few days after the Navy's plan to reactivate its four *Iowa*-class battleships was revealed in detail last year during Congressional hearings, the letters started coming in. They read as if from a ditto machine..."I'm ready. Take me. I know that ship." The writers listed either *USS New Jersey* (BB 62), *Missouri* (BB 63), *Wisconsin* (BB 64) or *Iowa* (BB 61) herself.

The typical correspondent was in his mid-to-late 50s—a child of the Depression who grew up to sail off in righteous defense of his country in the most feared ships in the world. Now they are going back to sea, and he wants to cross a gulf of four decades to be there, too.

Insofar as modern military institutions can be touched by such gestures, the Navy was. But official reaction was tempered by the fact that emotions do not justify multimillion-dollar defense appropriations. What *does* justify the return of the battleship, according to the Navy, is a simple matter of cost efficiency in an increasingly complex defense environment. In the case of the

...more offensive punch than any other surface ship



New Jersey, which will undertake sea trials as early as next September, an outlay of \$326 million will put to sea a capital ship unmatched in offensive firepower by anything even on a drawing board, not to mention in a shipyard or afloat. The same sum applied to building a completely new ship, the reasoning goes on, might be sufficient to produce a

frigate, the least capable escort-type vessel, at today's prices.

New Jersey's sister ships, beginning with Iowa in 1985, will follow at approximately yearly intervals if the Navy gets its way. Reactivation costs for each of these will far exceed the New Jersey's price tag, since they will have to be brought to the latter's 1969 "baseline" configuration (during previous reactivation for Vietnam) before receiving the mix of missiles and defense systems the Navy considers crucial to the update.

Still a bargain, according to the plan, and the logic seems intact: Although the *Iowa*-class battlewagons were all launched between 1942 and 1943, they were, with the exception of *New Jersey*, permanently moth-



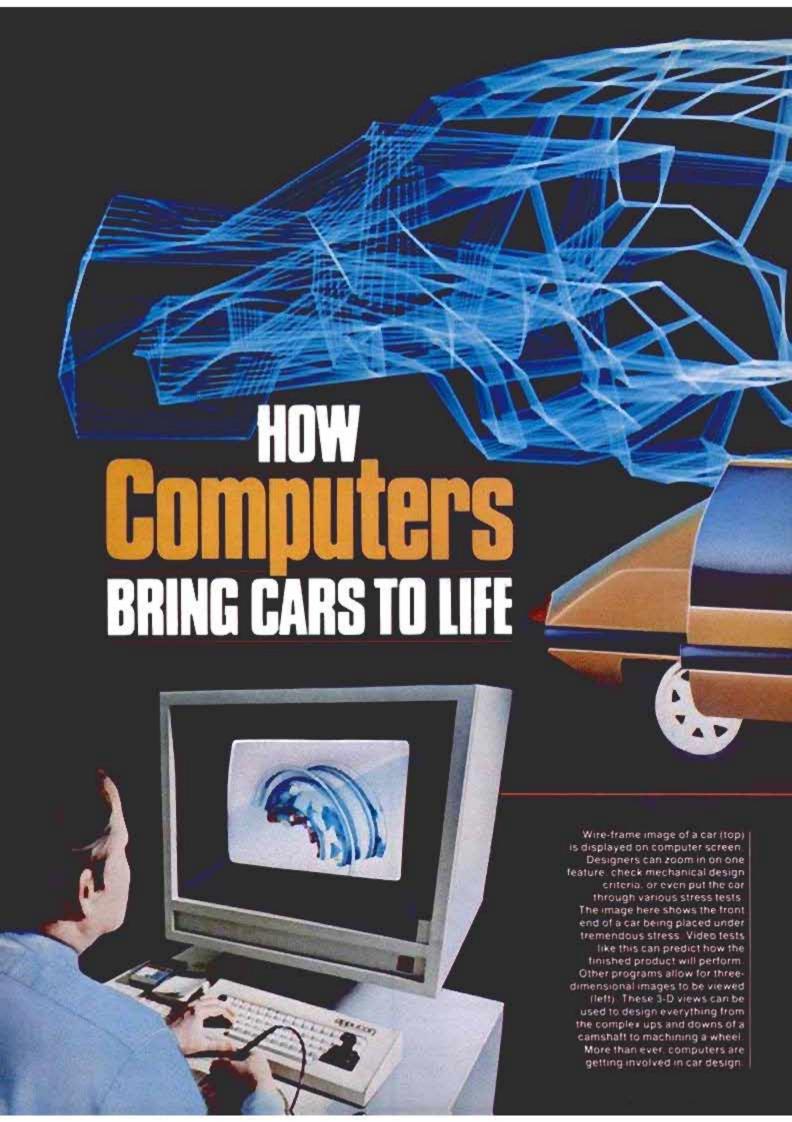
balled by the late 1950s. A ship's technical age is figured by time on the powerplant; even including New Jersey, they average only about 12 years' steaming and should look forward to a total lifetime of between 25 and 30. By that standard, curiously, the World War II battleships will emerge from drydock as some of the Navy's youngest ships.

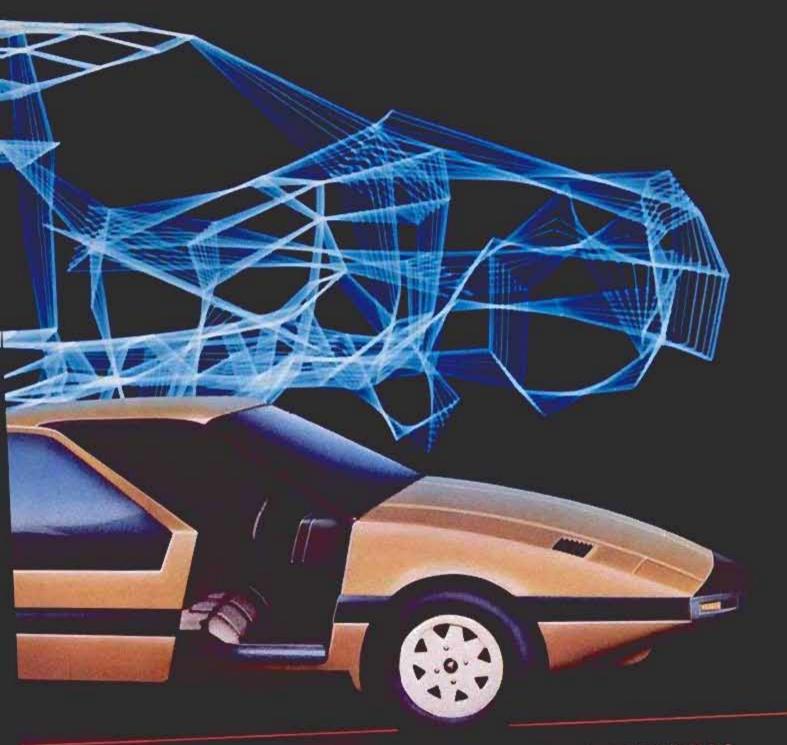
Yet it's impossible to regard a battleship—a dreadnought—merely as a relative bargain in steam boilers and armor plate. Before strategic airpower and nuclear bombs came simultaneously of age, this century's arms race knew only one doomsday machine, one balance-of-power weapon: the battleship.

In planning the original South Da-

kota class as early as 1916, the United States was determined to have a ship that could control the seas against whoever won World War I—whether it was Germany or Britain—and cancellation of it short of completion in 1921 was as ironclad a guarantee of peaceful intent as a nation could offer. Naval treaties limit-

Please turn to page 141)





Under the guidance of skilled humans who man the keyboards, computers are now designing the look, drive and durability of Detroit's cars of tomorrow.

by Neil Shapiro ELECTRONICS EDITOR

o you and me, a car is a symphony of curvaceous metal, of touchy but stalwart machinery-all wrapped up around a little bit of soul. But to calculating computers deep in the heart of Motown, a car is nothing more—or less—than a collection of 20,000 points to be plotted and replotted, twisted and displayed as ghostly, video images on flickering

It's all part of Detroit's ongoing commitment to

CAD-Computer Assisted Design. The shape of many of the cars you'll be driving tomorrow, or even today, is influenced by a computer, guided by human hands, eyes and feelings.

As Frank Biondo of GM Design put it, "We can't

forget the human areas.

And human hands still shape the clay from which automotive dreams are made. Full-size clay models-design mock-ups-are exact replicas of what the designers first envision as the finished car.

Just walk through the basement areas of GM's

main design division in Detroit and you'll catch an occasional whiff of clay seeping out from behind locked, no-admittance doors.

The smell of clay is the odor of mystery. Within those secret chambers, the plans and dreams of the corporate car giants begin to take physical form. Smoothed and finished by loving hands, with glossy plastic sheets applied tightly enough to clay sides so that they appear to have coats of shiny lacquer, the clay models wait for the approval of both man and machine.

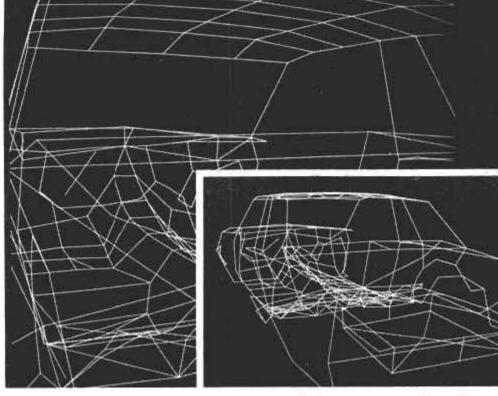
A mechanical scanner, taller than a tall man, but with the delicate touch of a young girl, is rolled into position beside the clay model. The mechanical arm of the scanner reaches out, traces the graceful curves one by one and converts each aerodynamic sleekness, every stylistic bump, into a series of coordinates. Then these coordinates are stored as binary bits on a floppy disc that, to a computer, is more readable than the glossiest clay surface.

From there, the wet smell of clay becomes a background memory to the airconditioned, office-clean cubicles of the design-team people.

The stored points are instantly accessed by desktop computer terminals as clay is converted to video. A car appears on the screen: It's tilted, moved around and examined in detail until every part of the dream can be certified as being worthy of reality. Of course, reality to GM or Ford may differ somewhat from our own consumer-oriented perceptions of a car.

On-screen wizardry

On a recent trip to Detroit, we watched anonymous video cars being put through their computerized paces on monitor after monitor. It was a combination of the Indy 500 and a pinball fantasy. But it was all deadly serious.



On-screen views allow computer-assisted designers to look at every structural part of a car before it's built. The scale may be zoomed in or out. Every part is seen in relation to every other part, so effects of simulated stress or design change can be observed.

The first test we watched involved a procedure as human-oriented as packing your trunk for a long trip. The video display zoomed in, under the guidance of the designer, and became a full-screen view of a car's open trunk. The screen beneath the trunk was filled with a list of options, such as "golf bag" and "lady's overnight."

The designer touched his light pen, like a magic wand, to one of the on-screen choices. Instantly, a golf bag appeared, as transparent as the car it would soon be packed in.

With the light pen, he positioned one piece of luggage at a time. Everything fit. But if it hadn't, it would have been back to the clay for redesign and further study of the car's understructure.

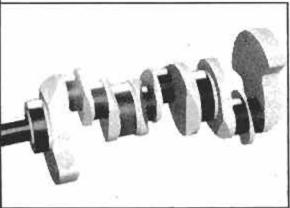
If necessary, the clay model's

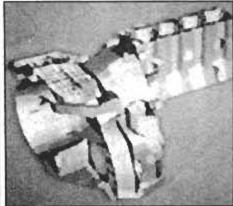
trunk could be widened, the hinges redesigned, or room could be added somehow to achieve what the computer-aided designers consider satisfactory space. Any changes would require careful scraping of clay, accompanied by new measurements to be turned into computer coordinates.

Phantom rider

Then we watched as, back on video, a computer's version of a human being took the driver's seat. He looked for all the world like one of Casper the Ghost's best friends.

This time, the light pen was used to choose options that detailed the amount of legroom, ankle flex and other human-machine measurements that drivers of various sizes could expect in the car as designed.

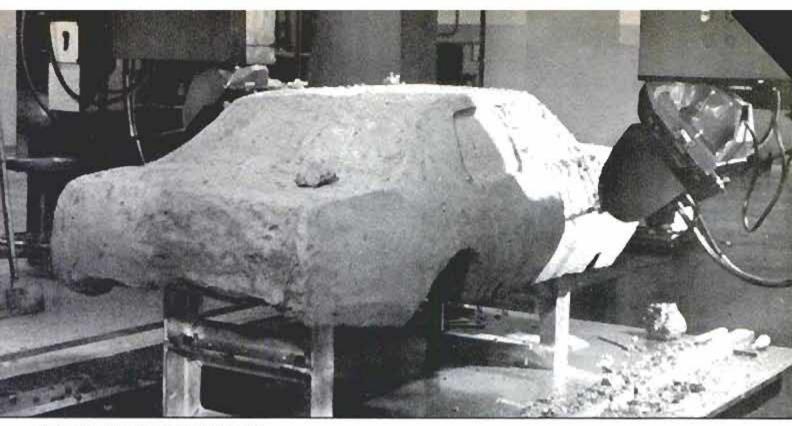






Three-dimensional images are just now being introduced to the computerized design teams. Camshaft (left) was designed by new GM-Solid program that allows for realistically detailed and shaded imagery. Ford also uses three-dimensional techniques. This pro-

gramming lets the computer indicate visually the stress patterns in flexing metal. Spring (right two photos) is shown before and after computer-simulated stressing. Changes in image color indicate to designers what would happen to metal.



Mechanical arms scan clay models and translate model's stylistic detail into coordinates that can be computerized.

A range of people, big and small, stout and thin, could be called on for video duty.

In the old days, the designer would have had to depend on plastic models of people. These plastic likenesses, hinged to move in the same manner as humans and slotted to be adjustable in size, were laboriously compared to diagrams, blueprints and maybe concepts scribbled on a napkin, in order to determine human/machine compatibility.

Now, without ever having built the car, the designer can talk over its comfort features with the auto's first "driver."

"Without computers," GM's Frank Biondo told us, "the design process would be far more time consuming. The computer is helping us primarily in the definition of design. We're confident it delivers a far greater degree of reliability and value."

Everything on the car is tried out on video. Will the door swing open properly, or scrape against normalheight curbs? Or will it pound the pizazz out of the car's painted fender? Do the seat backs have sufficient rake? Does the hood open wide enough to allow the engine to be worked on?

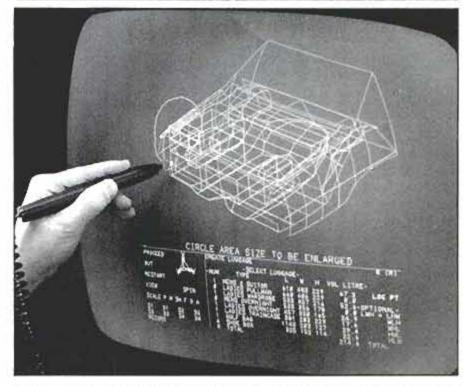
Then, when the design is complete artistically, it must be re-examined structurally. The cleanest, smoothest lines don't guarantee a heavenly ride or good gas mileage.

The points whiz from one computer to the next, from the design people to the ones in charge of man-

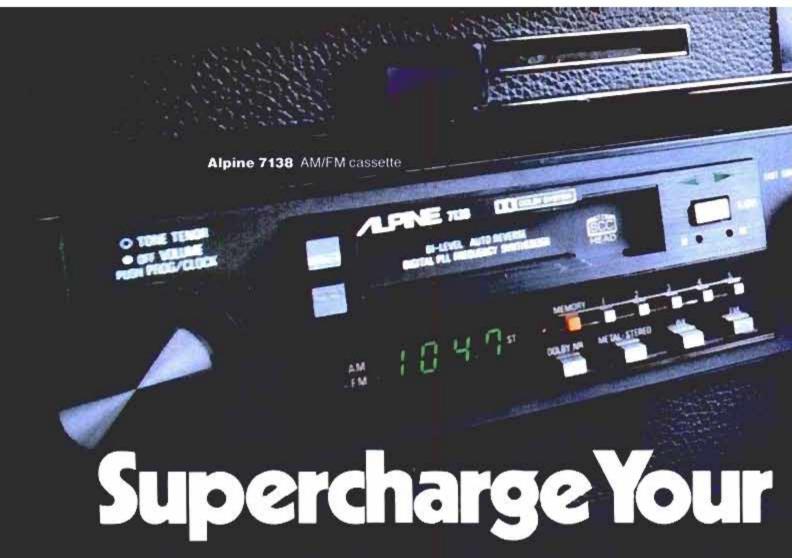
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Performance simulation (top left) allows Ford designers to drive a design idea around a racetrack to get feel of a finished car. Legroom and other car-to-human measurements are investigated at GM by a program (top right) that lets the designer see how car will fit driver. Luggage packing program (above) helps design GM's trunks.



Installing all the car stereo equipment above takes a great deal of patience—as well as the right tools. At right, we show two useful tips. First, purchase a crimping tool. It lets you attach terminals to stripped cables quickly and easily. And don't forget heat-shrink tubing. Applied to bare wire, it shrinks into a tight, insulating sheath with the heat from a match.





Physical Toylor,

If you already have a good, basic sound system in your car, fear not—you can always upgrade it into a rip-snorting, on-the-road system.

by Gary Stock

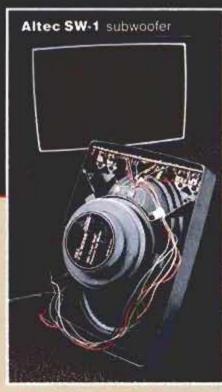
There comes a day in the life of every car-stereo owner when his basic sound system—usually an AM/FM cassette unit and a pair of speakers—just doesn't excite anymore. It may lack power, or a solid low end, or "shimmer" in the treble or even a few desired control features—but, somehow, the thrill is gone.

On that day, you have two options: Throw out the whole system and start from scratch on a new megabuck setup, or use your existing equipment as you upgrade, adding new components and accessories, one by one.

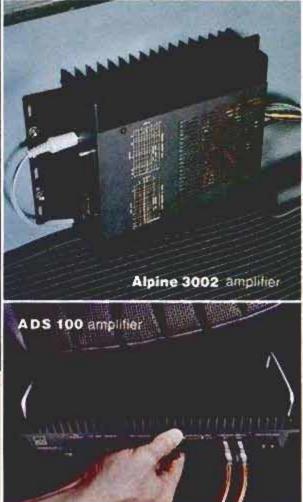
To illustrate this second approach, we took a well-known sports GT car-a Mercedes-Benz 450SL roadster equipped with a good, basic sound system-and subjected it to a full range of sound-system improvements. The owner of the car had outfitted it with two excellent components which served as our starting points: an Alpine 7138 full-digital AM/FM cassette unit and a pair of 41/2-inch Blaupunkt full-range speakers, fitted into the small speaker enclosures at the sides of the dash. The sound from this combination was smooth and well

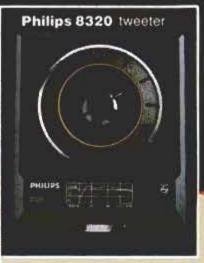
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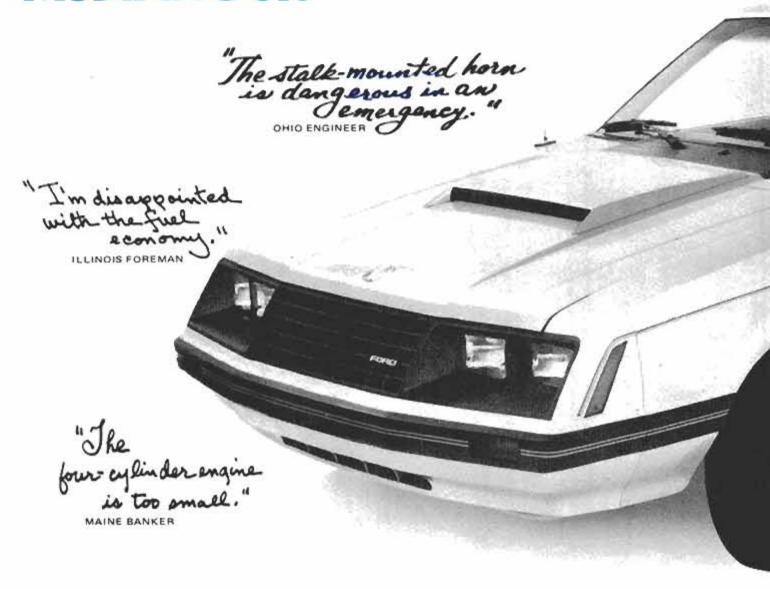
The Altec SW-1 subwoofer (above) includes all of the electronics on same chassis as the speaker cone. A subwoofer allows you to take that bone-jarring sound of real bass along wherever you roam. The tuning controls mount separately in the dashboard area.





Philips 8320 tweeter (above) was mounted on each Mercedes door. Placing them forward on doors protected them from slamming impacts. Alpine's 3002 power amplifier (left, above) was mounted in trunk space. It can also be mounted on firewall. ADS 100 power amplifier (left, below) was hidden beneath the driver's seat. Make certain (as shown) that the unit will fit with adequate clearance before you begin the actual installation.

MUSTANG 5.0



Styling and handling satisfy pony owners.

A ccording to owners, the 1982 Mustang goes a long way toward satisfying their urges and doing the things expected of America's original pony car.

Styling came away with top honors and generous praise from the owners we interviewed—both as a reason for buying the car initially and as a source of long-term satisfaction after purchase.

A West Virginia student beamed, "Styling and fuel economy sold me. My car is black with three shades of orange, which gives it a beautiful



Many owners complained about the stalkmounted horn button and rattles in dash.

appearance. I also like the fuel mileage; I'm getting 24/30 mpg with this four-cylinder four-speed."

A cola bottler from Georgia told us, "I wanted a small car, but not too small, and I liked the looks of the Mustang." And a North Dakota sergeant admitted, "Appearance was the overall reason we bought the car."

An Ohio financial vice president confided, "I didn't want a box on wheels." A Michigan realtor added, "This Mustang is a sporty car and more fun to drive than larger models," and a New York secretary smiled, "I've had a lot of people comment on my Mustang's good, clean, sporty looks."

Handling ended up a close second behind styling among the Mustang's best liked attributes. You can order any new Mustang with the TRX handling package, but some fea-

A NATIONWIDE SURVEY BASED ON 802,115 OWNER-DRIVEN MILES



PM priotos. Jerry Abramowitz

tures, like alloy wheels and Michelin TRX low-profile tires, along with recalibrated springs, shocks and stabilizer bars, are mandatory.

A Kentucky hairstylist told us, "On the road, it clings and holds fast around curves; is also good on ice and easy to maneuver through traffic. Yet despite the sporting suspension, the ride isn't too stiff, so it's a comfortable car for long trips."

Fuel economy came in for mixed reviews, some owners praising their cars and others telling us they'd expected better. Economy placed among the Mustang's best liked qualities with 26 percent of our respondents, while 15.2 percent rated it below expectations.



The optional 5.0-liter V8 engine makes the Mustang the quickest car on the market.

Most fuel-mileage complaints came from owners with the four-cylinder engine and automatic, and so did gripes about engine power. An Illinois production foreman wrote, "I bought this car for good gas mileage, but I'm averaging barely 18 mpg around town. The EPA said I should expect 22 mpg. The engine doesn't have enough power to get out of the way of other cars and I'm disappointed with fuel economy."

A Maine banker: "The four-cylinder engine is too small. It won't go up a steep hill without the transmission dropping into low range."

And a North Carolina analyst wasn't alone when she observed, "If

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CAPTURE THE STARS

Taking beautiful astrophotographs need not mean a large outlay for equipment. Here's how to use an inexpensive telescope—or how to forego a telescope entirely.

by Robert Little

Almost all amateur astronomers — sooner or later — become sky photographers, and it's not hard to understand why. Many of the most beautiful sky objects can be seen only dimly—or cannot be seen at all—with even a fair-sized telescope.

Unlike the eye, film has the ability to build up an image during a time exposure and will record colors that the eye cannot detect at low

light levels. For instance, the translucent, purple veils of the Orion Nebula appear greenish when observed visually in a telescope.

Constellation photos can be taken with modest equipment—you don't even need a telescope (see Build This Star Photography Platform, at the

Long exposure times involved in capturing deep-sky shots, like Orion Nebula, call for manual or electronic guidance (electronic system shown). The astrophotographer must keep tracking motor on target.

end of this story). The moon, too, can be captured with only a camera and tripod. But if you do have access to a telescope, the whole universe awaits you.

Although the moon appears large in the sky, it's really a small object—photographically. A camera with a 50-mm lens would capture an image on film of only ½ mm in diameter—not—suitable for good-size enlargements. A telephoto lens of 300 mm.

however, would reveal many lunar features. (To compute image size on film, divide a lens's focal length by 110. So a 300-mm lens would give a lunar disc slightly under 3 mm across.)

The moon is so bright you can use a 1500-

lease from to parget 16

F/11 telescope was used with proper filters. Exposure was 1/250 second. Note sun's fiery corona around edge in the third photo-

This solar-eclipse sequence (below) shows some of the spectacular sights a skillful amateur can capture. An inexpensive 4-inch









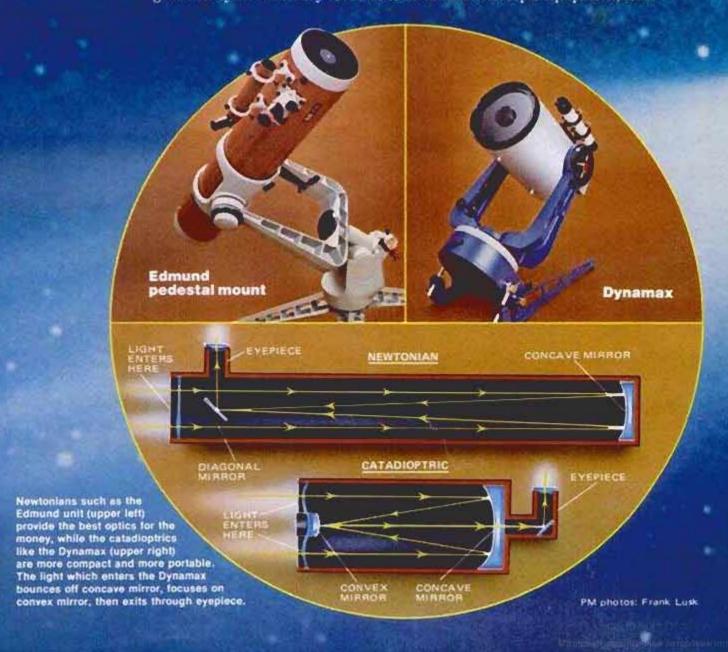


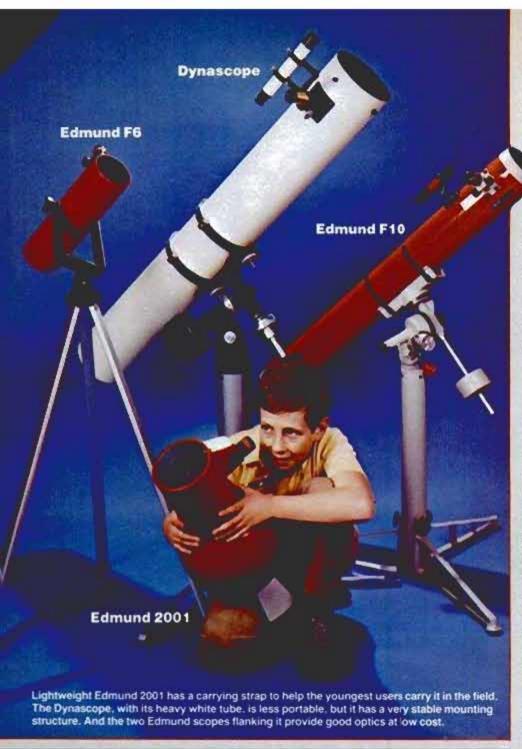
BACK-YARD JOYAGE TO THE STARS Visit meteor swarms, neighboring planets and distant galaxies for under \$700.

by Dennis Eskow SCIENCE EDITOR

asten your seat belts and get yourself ready: This month, the Earth begins a summer-long plunge through a sea of meteor chunks left behind by several streaking comets. As our orbit crosses paths with the meteor swarms, thousands of brilliant chunks of iron will flash across the evening sky, signaling the beginning of the summer meteor showers.

The spectacular nighttime display is enough to send you running to the local shopping mall in search of a hobbyist's telescope. But before you spend good money on what may turn out to be useless telescopic equipment, feast





	TEL	ESCOPE	SCORE				
Brand/Model Mirror Diameter	Price	Assembly Instructions	Operating Instructions	Mounting (tripod, etc.)	Weight (ibs.)	Eye- pieces	Camera Adapter
CELESTRON C-90, 5"	\$650	Very good	Very good	Excellent	22	3	Yes
DYNAMAX,	\$610	Very good	Excellent	Good	16	2	Yes
DYNASCOPE RV8, 6"	\$299	Good	Good	Excellent	50	2	Yes
EDMUND PEDESTAL MOUNT, 6"	\$479	Excellent	Excellent	Excellent	40	1	Yes
EDMUND F10.	\$369	Excellent	Excellent	Good	35	1	Yes
EDMUND F6. EQUATORIAL MOUNT, 3"	\$159	Excellent	Excellent	Fair	8	1	No
EDMUND 2001, 4%*	\$209	Excellent	Excellent	Good	10	1	No
MEADE REFLECTING TELESCOPE, 8"	\$549	Good	Very Good	Excellent	65	2	Yes

your eyes on the eight models tested by PM. In selecting our test telescopes, we tried to avoid the two biggest mistakes made by newcomers to stargazing: spending too little, or spending too much.

"Do you have any idea how many thousands of cheap Japanese telescopes are sitting unused in people's garages?" asks New York Amateur Astronomy Society president John Marshall. "Thousands of them are sold every year it's criminal."

Cheap is no bargain

The scopes Marshall and others avoid are those slender tubes sold for under \$100 in the camera departments of big stores. No bargain, they generally have inferior optics and they frustrate the new user.

The instruments we tried are all high-quality reflector telescopes. Each is basically a mirror mounted inside a tube. Light from a heavenly object hits the mirror, is focused on a facing mirror and angled out an eyepiece to the stargazer. We avoided refractor scopes—the long, skinny types (with lenses, no mirrors) a sea captain might use in search of a landfall, but never in search of a planet. Good refractor telescopes, for astronomy do exist, but the optics are far more expensive than the mirrors in scopes we tested.

PM found excellent telescopes for no more than \$650 and as little as \$159. Each had advantages—and disadvantages.

For instance, we focused the Meade 8-inch scope—which provided the most spectacular sky show—on what appears to be a single point of light known as "iota" in the constellation Cassiopeia. With the Meade 8-incher, we could see that three bright and distinct stars make up the apparent point. The Meade also gave us breathtaking views of the rings of Saturn and the cloud bands of Jupiter. Its biggest disadvantage was weight and bulk—it weighs 65 pounds and has a tube 50 inches long.

Celestron's 5-inch mirror couldn't resolve the triple star clearly, but it showed us a nebula—or "clouds" of stars—in Orion and yielded good detail of Jupiter's cloud bands. Still, it did have one advantage over the Meade. The C-90 is a catadioptric scope, meaning its optics are arranged in a compact tube. It's easier to carry around than the Meade and other long-tubed Newtonians.

The other catadioptric we tried, the Dynamax 6-inch model, focuses on more outer space details. But our model's mounting structure was a

(Please turn to page 172)

PM's Long-Term Car Tests

Our new durability testing program will put on the miles to learn how some popular makes stand up to the rigors of the road.

by Tony Assenza AUTO EDITOR

There's nothing quite as wonderful as the feel of a brand-new car. It smells good, everything is tight—usually—and everything works. But what happens when the engine has a few miles on it and the chassis is subjected to thousands of miles of bumps, potholes and highway expansion joints? And does that wonderful paint job start bleaching after its first month in the summer sun?

These are the kinds of things we can't find out in our regular road tests. After purchase price and fuel mileage, the next thing prospective car buyers want to know is how well will their new car stand up. We've never been able to fully answer that question before. Now, our long-term test program will help remedy that situation.

To better furnish you with the kind of information you need to make a wise buying decision, we've embarked on an ambitious durability testing program. Obviously, we cannot test every car on the market.

The next best thing is to select a group of cars which we feel is significantly interesting to you—the types of vehicles you'd most probably consider buying—and drive them for thousands of miles under a variety of road conditions.

We'll be keeping careful records of mileage, oil consumption, mechanical problems and general durability. We'll also be maintaining the cars ourselves so, hopefully, we'll be able to give you some insight on serviceability.

Our first group is a varied lot from a simple economy car to a thinly disguised hot rod. Here are the cars and how they stack up.

Plymouth Horizon

Remember our 'Real People' Road Test Of Economy Cars (page 65, Aug. '81)? We're still scarred from the withering barrage of letters that poured in following our report on the







Omni Miser. If you recall, we weren't exactly fond of that car. And you, along with some powerful Chrysler executives, weren't everly fond of us for ranking it dead last in a field of 10 competitors. So, we decided to take another look at the Dodge Omni (the Plymouth Horizon is the same car with a different grille).

We picked up our test car in Detroit on January 16, 1982. The car had 582 miles on it. It was a deluxe model, loaded down with every comfort and convenience option in the Plymouth book—airconditioning, deluxe cloth interior, automatic transmission, AM/FM stereo, cruise control, rear defogger, bucket seats, console and more. List price of the car came to \$8,595, although you can buy any car at less than list price.

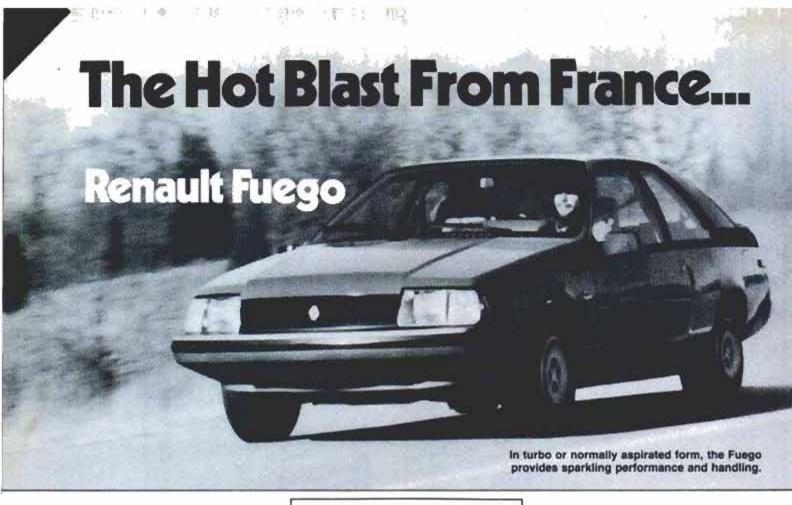
Upon receipt of the car, we immediately started driving it to New York via Canada—in one of the worst blizzards of the year. Thanks to the car's 2.2-liter engine, sport suspension and Michelin XZX radial tires, we found the car very stable and sure-footed, despite the dastardly weather. Except for a minor off-road excursion involving a jack-knifed tanker full of deadly chemical waste, we were able to press on with vigor and still managed to get 22.3 mpg for the trip. Really respectable.

Okay, Chrysler, we're not exactly eating our words on the Miser-nibbling just a bit, maybe—but this Horizon is certainly heads and shoulders better than the little gobbler we got the first time.

The car was put into regular commuting service upon its arrival in New York and has so far accumulated 3.050 rather uneventful miles. At 2.654 miles, the front spoiler fell off as we were pulling into a parking lot in Atlantic City, N.J., to attend an old-car auction. At 2,834 miles the engine suddenly developed a heavy thirst for oil and gulped down two quarts at a routine gas stop. The car had not used any oil previously, and has not used any since. It must have been the culture shock of being piloted through the asphalt warfare of New York traffic that scared it into ingesting the two quarts of 40weight lubricant.

Despite the heavy load of options, the 2.2-liter engine in our test car powers out the Horizon with brisk acceleration. Zero to 60 happens in 10.9 seconds, shifting the automatic manually. We've surprised many so-called sports cars with the Horizon's excellent performance and handling. It's great fun to blow off these pretenders on freeway on-ramps. They never quite believe that they're being outcornered and outaccelerated by a Plymouth Horizon.

We've been driving this car ex-(Please turn to page 149)



FIRSTHAND REPORT

or those of you—and you know who you are—who have been calling us about advance information on the Fuego, the word is pronounced Fwaygo. In Spanish it means fire. In French it means an exercise in technologically advanced motoring. And in America it will most certainly mean speeding tickets for those who explore the Fuego's full potential. We know, because we had the opportunity to drive American-specification Fue-

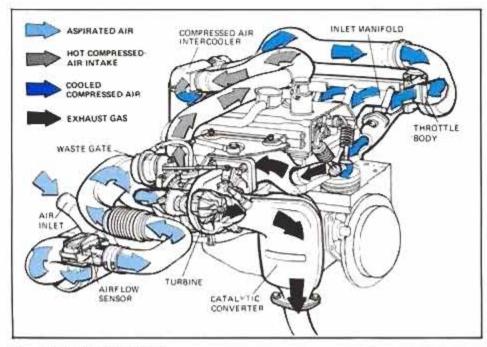
by Tony Assenza AUTO EDITOR

gos on the streets and high-speed autoroutes of northern France.

Ordinarily, driving Americanspec cars in Europe is an open invitation to being humiliated by domestic cars with half your displacement. Since they have fewer emissions controls, European engines generally make more horsepower than ours and get better fuel consumption.
And the average European driver
misses few opportunities to make
full use of his available ponies.

This was not the case with the Fuegos. This time, the contest was more evenly matched.

On a run from Paris to Caen—exuberant about driving the turbo for the first time—we averaged a shade over 80 mph, which meant the engine spent most of its time near the red zone in fifth gear. The mile-





Even in base trim, the Fuego comes well appointed. It has excellent bucket seats, thickly padded wheel and lots of gauges.

The 1.6-liter turbo engine has an air-to-air intercooler and an integral waste gate.

age was nothing short of amazing-20.78 mpg for the trip. At more conservative speeds, it would easily

have gone over 30 mpg.

All the time and money Renault spent on developing the aerodynamics of the Fuego begins to pay interest at these elevated speeds. Wind roar is hardly more noticeable at 100 mph than it is at 60. Another dividend of the flying hunchback look is the amount of downforce created at speed. Cars with unfavorable aerodynamics tend to lift their front ends as speed increases. This reduces road feel and gives you the impression that the front wheels have the precise control of a couple of chocolate frosted doughnuts.

The only directional stability problems we encountered with the Fuego was in a stiff crosswind as we crested an arched bridge. Apparently, air got underneath the car and tried to make it change lanes.

On the twisty back roads of Normandy the car felt as good as it did on the wide-open autoroute.

Kicking up rooster talls

The highway system in France is really good, for the most part, but the rural roads are something else again. You could be on a two-lane blacktop, power through a corner and suddenly find your front wheels kicking up rooster tails of red topsoil and the car power-sliding its way into the local Stella Artois beer dispensary. A hundred yards down the lane it could be cobblestones or dirt. or a combination of road surfaces.

For the uninitiated, this could play havoc with your nerves. But, since the Fuego is a native son, it wasn't overly upset by the sudden

change of road surface.

The Fuego is available with either the normally aspirated, 1.6-liter Four or the turbocharged version of the same engine. In either case, the fuel-delivery system is by Bosch L-Jetronic injection. The nonturbo engine is rated at 81.5 hp, while the blown engine is good for 107 hp, both happening at 5,500 rpm.

Like the Audi Quattro, the turbo Fuego engine is fitted with an airto-air intercooler which reduces the temperature of the compressed air after it exits the compressor and before it's fed into the engine.

As is the case in the Quattro, the intercooler allows for stingier fuel metering and optimizes the power available from the turbocharger. In its transformation from normally aspirated to turbo version, the compression ratio is reduced from 8.6:1 to 8.0:1.

Throttle lag, a problem common to

(Please turn to page 165)

... And One From Kenosha



Although it looks a bit like a toolbox, the Renault 9 is nonetheless pleasant and fun to drive. The U.S. versions will have recessed headlights and 5-mph bumpers.

nlike the Fuego, the Renault 9 will be built in an AMC plant in Kenosha, Wis. The first U.S. car has yet to roll off the line (it'll go on sale this fall as an '83 model), but we did have a chance to drive the European version in Normandy.

Where the Fuego is more roadscorcher than transportation, the 9 (still unofficially referred to as the Alliance) is utilitarian, yet competent and a satisfying car, nonetheless.

It's powered by a 1.4-liter Four going through a four- or five-speed manual or an automatic transmission, uses front-wheel drive and, uncharacteristically for Renault in this class of car. has a trunk rather than a hatchback.

The French engine, with no pollution-control devices, cranks out 72 hp. which felt adequate for most of the driving chores the 9 would be expected to perform. In American trim, the engine is expected to produce horsepower in the high 60s and yield in-town mileage of around 30 mpg. Final tuning is still to be determined, but we have it on good authority that 1 or 2

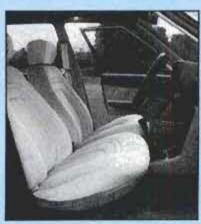
mpg may be sacrificed for better performance-a sensible approach.

AMC engineers and assembly people have been shuttling back and forth between Doual, France (where the European 9 is built), and Kenosha, in an effort to make sure that nothing is lost in the translation.

Both AMC and Renault are concerned that the U.S. versions be as lovingly screwed together as their European Car of the Year winner for 1981

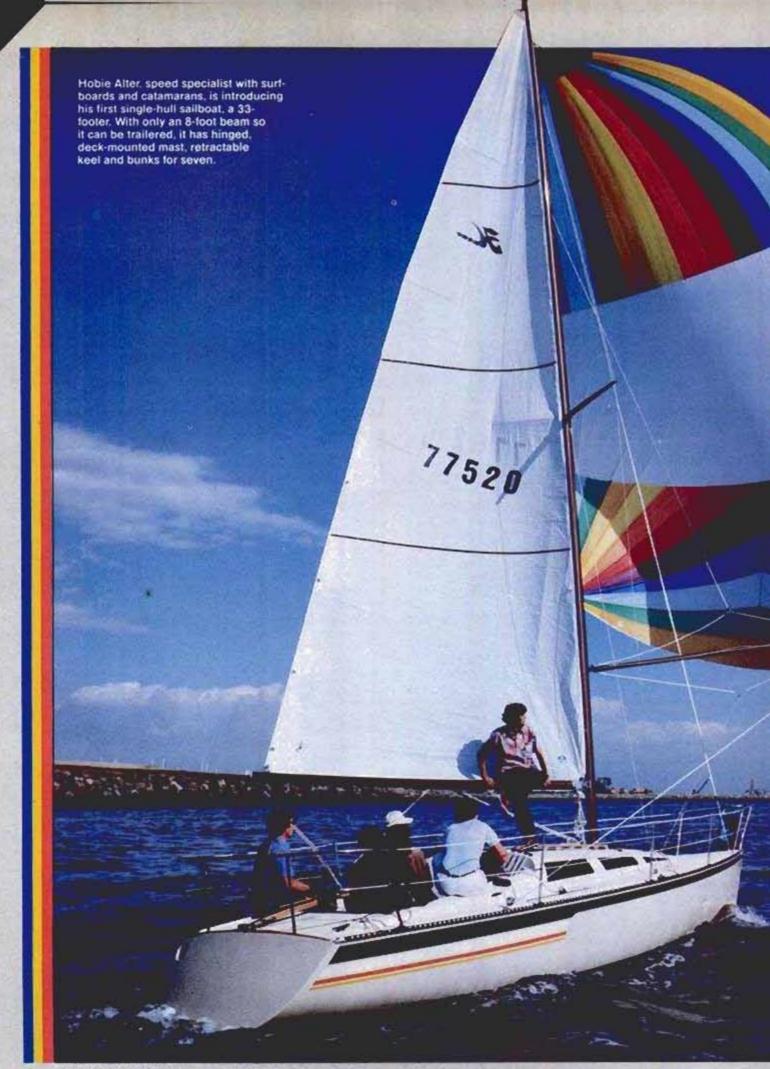
One unique feature of the 9 is the Monotrace seat mount. Instead of the usual two-rail subframe, it has a central pedestal which allows the seat to rock fore and aft for adjustment. This saves space and leaves more room for rear seat passengers' feet.

The 9's most attractive feature to me is the ride quality and that all-important, but undefinable quantity, "feel." The car feels right-stable, precise, compliant, but not billowy. I know AMC will try to "tailor" it for American tastes (i.e., softer ride), but I would leave it alone.-T.A.





Interior of the 9 features Monotrace seats (no tracks) as well as a surprising amount of room. The fit and finish quality is quite good and the dash is very well instrumented.



New Look For No-Fuel Boating

ree, boat designers keep trying to use it more efficiently. This
summer, twin-hull sailboat
makers are testing singlehull performance while
mono-bottom powerboat builders are expanding sailing catamaran lines. Sailing
surfboards are growing
in popularity, with oneand two-hull models
available. Even inflatable sailboards are here

Mono and multihull designers seek more speed on wind power.

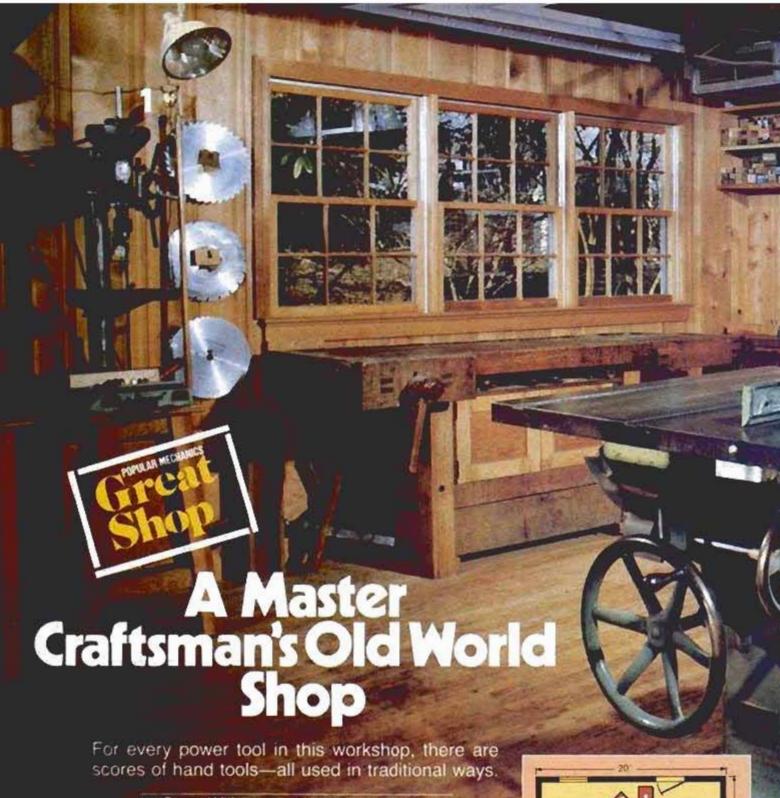
by Bill McKeown

Hobie Alter, originator of the famed Hobie Cats, has introduced a racer-cruiser sailer that answers a number of expensive problems like: How do you find and pay for deep-water dock space? How do you fit a large family or a lot of friends aboard, yet sail other times alone, or with a minimum crew?

With the new Hobie 33, you don't need a slip. The big 33-footer is only 8 feet wide so it can be trailer-towed without a special permit. The mast is stepped in a tabernacle on deck so that it can be hinged down or raised without a hoist. Roller reefing aids single-handling. Halyards are led down inside the mast. A compartment aft houses an outboard motor that can be tilted down for auxiliary power. Below, in the cabin, the galley

(Please turn to page 164)





by Steve Willson ASSISTANT HOME AND SHOPEDITOR

he latest installment in our Great Shop series has much in common with its predecessors-and something a little different. This time, we tried to focus a bit more on the man behind the tools, and with good reason: His traditional training and old world devotion to craft are more rare and remarkable today than they have ever been.

Ernest Glaser is a master furniture maker schooled in Germany during the early 1920s under the old European Guild System. He came to the United States in 1928 and began work in cabinet shops around the

- 1 Glaser converted a smokehouse (inset) into a comfortable shop. He reinforced the floor and roof, installed bigger windows and added maple flooring and pine boards to interior. As floor plan at right shows, shop is compact, but has plenty of room.
- 2 Antique toolbox holds seldom used tools, leaving wall space free for others.
- 3 Shop has nearly 200 different clamps, including bar clamps on a special rack.
- 4 Hardwood brackets hold over 40 handscrew clamps within reach, but out of way.
- 5 Razor-sharp carving chisels are used for fine-detail carving, precise inlay work.



SHOP FLOOR PLAN

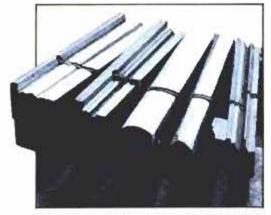
- Band saw
- Grinder
- Jigsaw 5 Jointer
- Drawing table
- Table saw Lathe
- Shaper
- 10 Warkbench 11 Drill press

Workbench





A Master Craftsman's Old World Shop



Shop has collection of 40 wood planes that Glaser still uses to cut rabbets, plow dadoes and form intricate molding shapes.

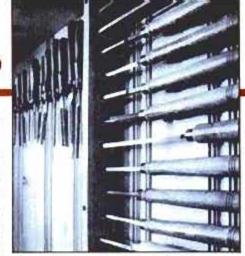
New York City area. By 1945, he earned the B.S. degree in education from Rutgers University and began teaching woodworking in high schools near Plainfield, N.J. He stayed there until 1964, when he and his wife Elizabeth retired to the Eastern Shore of Maryland to begin

a life that was anything but retiring.

Along with refurbishing a huge, two-story house, he gave an old smokehouse a structural and cosmetic face lift, turning it into the great shop shown here. From the maple floors to the knotty pine boards on the walls, he realized his dream: to create a shop where he could make fine furniture as he had been trained to do.

When we arrived to see his shop, Glaser confessed that he'd spent some time cleaning up for us: A working shop is rarely as clean as these photos show. He had four projects in the works, three on commission. The first was a series of six identical boxes made of ash with rosewood inlay. Each had a handrubbed finish and showed off Glaser's considerable expertise in making hand-wrought dovetail joints.

The second was a five-drawer mahogany silver cabinet that stood about 42 in. high. It was completely



Heavy-duty carving chisels (left) hang next to turning-tool rack. Make the rack by boring handle holes half through one support, blade holes completely through other.

sanded, but not yet finished or rubbed.

The third was a small chest of drawers in the Virginia style—mahogany again—that was just being readied for shipping. And the last project was for his personal use: a series of four Chippendale side chair reproductions. He used these unassembled chairs to illustrate how he builds a piece of furniture.

His starting point is his drawing board, where he sketches a design, altering it here and there until he is satisfied. For the chairs, this was unnecessary, because he wanted to match some existing chairs. Next, he makes a full-scale drawing which, he says, is crucial. "The eye can play tricks in the smaller scale. It is only when the piece is full size that you can know with certainty how it looks."

After any final design adjustments are made, he cuts full-sized patterns from cardboard or 1/4-in. plywood. He admitted that this was a time-consuming job, but nonetheless recommended it. These patterns eliminate mistakes in measuring and they're also a perfect record that can be used later to duplicate the piece without repeating the design work. Finally, he traces the patterns onto stock and proceeds with the cutting.

As a rule, he performs his joinery by hand. He enjoys the work and is so quick with it that, in many cases, he could rival power equipment if machine setup time were included in the comparison.

One of Glaser's favorite woods is African mahogany. Many of the pieces in his home, as well as those in photos of commissioned work, were made of this wood. Most of it came from one lot of mahogany he purchased over 30 years ago. It was, without a doubt, the best looking mahogany I have ever seen. The grain was extremely tight and the



Glaser (left) shows author the jack plane he made over 50 years ago. Journeyman's Certificate (right) was his after he had completed a Guild-approved apprenticeship.

The European Guild System

mest Glaser was born and raised in Hamburg, Germany. When he was 16 years old, his mother signed a legal contract, committing him to a four-year apprenticeship in the cabinetmaking trade. During the term of his contract, he was responsible to just one man, his master. He lived at home and worked in the shop five days a week; on the sixth, he attended a Guild-sponsored school where he learned drawing and mathematics.

When he completed his apprenticeship, he was awarded a Journeyman's Certificate (above), which qualified him for cabinetmaking work. By this time, the Guild no longer required of its members a literal journeymanship. But his master had told so many inspiring stories of his own travels that Glaser chose the traditional path.

For the next three years, he traveled by foot throughout the German country-side, working in various cabinet shops along the way. He was bound by two rules: He could stay no longer than four weeks in each town and he couldn't venture within 30 miles of his home.

Glaser did confess that he sometimes stayed longer than four weeks in one place, but he didn't go home until the three years were complete. And to this day, he's convinced this was the best way to learn his craft.—S.W.



Each room in Glaser's huge, two-story house is filled with his work. Sheraton-period dining table here is an example.

color was deep and rich. He still has some left for special projects, stored in another building with the rest of his hardwood stock.

A craftsmanlike approach

At one point in our conversation, Glaser mentioned that he preferred to work alone in his shop, without distraction from other people. "To do this work properly," he explained, "requires great concentration."

Later, he took me on a tour of his home. Walking through the house was like taking a stroll through a museum. All the rooms were filled with furniture of the highest workmanship—both his reproductions and his original designs.

One of the most remarkable pieces was a desk he made for his wife. Almost self-consciously, he explained that when he was a young man and he became engaged to his wife, he didn't have the money to buy her a ring. So instead, he built her a desk. It is in the Hepplewhite style and has seven, virtually undetectable secret compartments incorporated into the design.

He proudly showed me how the piece "worked," taking off a decorative molding here and there to expose a hidden drawer or a narrow compartment. While admiring his workmanship, I suddenly realized that the piece of furniture, though nearly 50 years old, looked as if it were brand new. There were no cracks in the finish, no rubbing on any drawers, no obvious signs of wear. When I remarked on this, Glaser looked completely surprised, as if my idea of what furniture should be was different than his. He seemed to think it should last forever.

Do you know a shop that we should include in the Popular Mechanics Great Shop Series? If so, send snapshots plus a description of shop and owner to Home and Shop Editor, Popular Mechanics, 224 West 57th St., New York, N.Y. 10019. If we agree, we'll visit the shop, photograph it and share it with PM readers. (Enclose a stamped, self-addressed envelope if you want your photos returned.)



Scrub, jack and smoothing planes (left to right) are used for surfacing rough stock.



Plane to correct thickness with scrub.



Scrub removes stock fast, leaves grooves.



Use jack after scrub; use smoothing last.

3 Planes To Make

n the "good old days" before power equipment, there was only one way to surface a board: by hand. Without jointers, planers and lumberyards that sell their wood already dressed, most of us would be lost. But when you have the proper hand tools and the know-how to use them, the job is far from imposing. In the photos at left, Glaser shows us how.

He begins with a piece of 1-in.-thick pine stock and a scrub plane, a narrow tool with a convex-edged plane iron which cuts deep grooves in a board's surface. It removes stock rapidly, so care must be taken to set the blade depth properly.

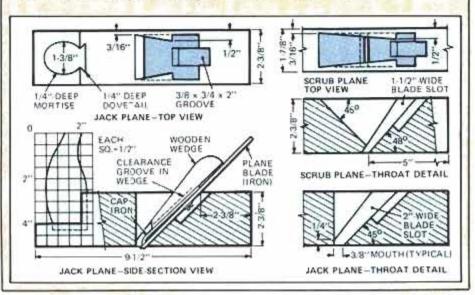
Next comes the jack plane, a real shop workhorse. It quickly removes the ridges left by the scrub and brings the board to desired thickness. Finally, he uses the smoothing plane to remove irregularities left by the jack plane.

In just a couple of minutes the board is planed to a uniform ¾-in. thickness and, though this sample is soft wood, Glaser says that as long as the iron is sharp, it will work just as well on hardwood.

Both the jack and smoothing planes pictured were made by Glaser over 50 years ago. Plans are shown below.

The jack plane is diagrammed completely; the others are variations based on this style. Because the smoothing plane is identical to the jack—just one inch shorter—it is omitted from the drawing. The scrub plane, however, is different and the appropriate section and top views are provided.

Straight-grained white beech is the preferred material for plane-making. After the plane bodies and 7° wedges are made, they are soaked in linseed oil for four to six weeks to harden them. Plane and cap irons are available in assembled form from tool suppliers like Woodcraft, 313 Montvale Ave., Woburn, Mass. 01888.—S.W.





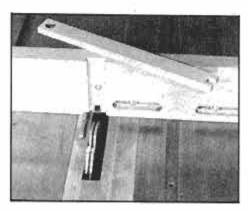
2 Shop Tricks From

ere are two nifty tricks that will make working in your shop even more satisfying. Master craftsman C.E. "Doc" Banister shows the secret of his modified finger-lap jig that will make you want to throw your old one away. Or, you may be able to incorporate this modification into your present jig. Another craftsman shows an easy way to make wooden dowel buttons.

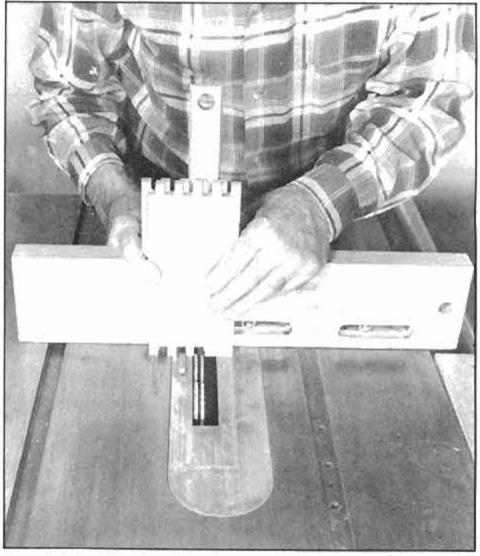
Antisplit Device

inger-lap jigs, or boxed-joint jigs as they are sometimes called, are among the most common jigs found in a craftsman's shop. Once you've used your jig for several different widths of fingers, however, you know that splitting begins to occur during the cut. This didn't happen the first several times you used the jig because the jig itself was serving as the backup board. The modification shown allows you to renew the kerf immediately behind the workpiece each time you use the jig. The hardwood backup-board insert is simply removed and the previous kerf area is sawed off.

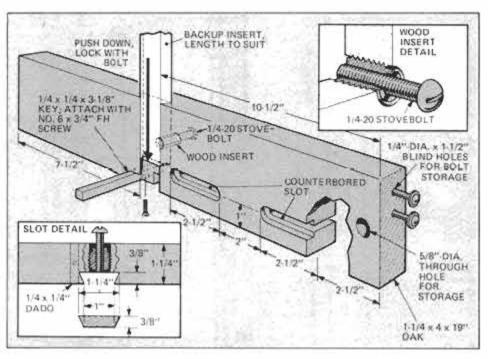
Look over the drawing and you'll see that the jig is very easy to make. Note that the finger gauge key is positioned so that it clears the slot. If you prefer your key closer to the kerf, you'll have to whittle off a bit of the back edge of the backup board each time it's renewed so it clears the key running through the slot. We include a few holes for storing the miter-gauge mounting bolts and the jig itself. The hole shown in one end of the backup-board insert is for the



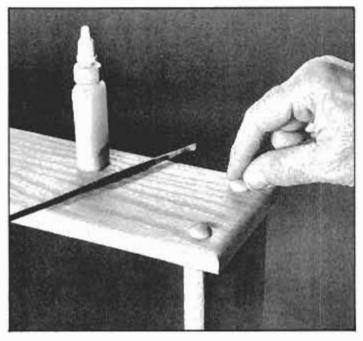
Pull backup board out after each session and saw off the length containing the kerf. Wood insert is visible in the slot.



The vertical piece behind the workpiece is a renewable kerf backup board. Jig is attached to miter gauge with bolts in slots. Finger gauge key is attached to bottom of the jig.



Master Craftsmen



Apply the carpenter's glue with an artist's brush. Insert the dowel buttons into the shank holes; press and remove any glue squeeze-out. You can also cut plugs by using the same general method we describe here. However, making your own dowel plugs or buttons can be so satisfying you may never buy either one of them again.

same purpose. Make a supply of these inserts while you are at it.

Cut the dovetail slot (and the inserts) at 20°. The inserts should be cut for a snug but sliding fit. Bore and tap the hole for the wood insert 1/4 in. below the slot centerline intersection. Install a compatible stovebolt or wingnut and use this system to lock the backup board in place. Wood inserts are usually available in hardware stores in sizes accepting 1/4- to 1/4-in.-dia, bolts or screws. If you have trouble finding them, write to Albert Constantine & Son Inc., 2050 Eastchester Rd., Bronx, N.Y. 10461. The 1/4-in, size used here comes in sets of 10 for \$3.50, plus postage; catalog No. is 119E25 .- C.E. Banister

Dowel Buttons

f you have had difficulty finding wooden buttons for your furniture projects, take heart. You can make your own with this system.

Here's how: Measure the diameter of the hole you wish to fill, then with the appropriate-size dowel, cut dado depth, using table below.

Put a strip of masking tape on the blade insert just in front of the blade. Set your dado head for a 1/4-in.-wide cut. Note that an adjustable dado head was used here. This cuts a dado wider than the tooth width because its design changes the blade angle.

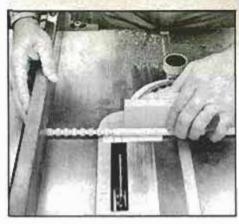
Make a test cut in scrap dowel and mark the tape with a pencil to correspond to gauge side of kerf. Make a second mark on the tape ½ in. toward the fence from the shoulder mark. Adjust the tooth height according to the table below. Set the rip fence on the line closest to it. Place the dowel against the miter gauge, allowing the dowel end to just touch the rip fence.

Start the saw and move the miter gauge to engage the dowel with the dado blade. Stop when the dowel is directly over the center of the blade (at its highest point). Then, using your free hand, rotate the dowel until a 1/4-in.-wide cut is made completely around the dowel.

Return the miter gauge and dowel to saw front, loosen the rip fence and move it aside. Slide the dowel to the new position by aligning as shown in the top right photo. Repeat the process for as many plugs as you need. Caution: Do not continue working on a length of dowel when the outboard, dadoed section becomes awkward and unmanageable. Use a backup board against the miter gauge, as shown in the pictures. Allow the board to support the dowel to within ½ in, of the blade.

Take the grooved dowel to a sanding disc and rotate it while feeding it at changing angles to round the tip. Use a fine miter saw or coping saw to cut off the button at the shoulder. Repeat the sanding and cutting process.—Dr. Robert N. Stapp

DOWEL PLUG TABLE (in inches)						
Plug Shank	Dowel Diameter	Dado Width	Dedo Depth			
1/4	3/8 3/8	1/2 1/4	1/16			



Begin each cut by lining up previous kerf with mark on tape. Set fence by moving it into light contact with dowel; lock fence.



With dowel held firmly against the miter gauge, feed it into blade, then rotate it. This is a delicate procedure; work carefully.



Round over buttons, using a disc sander as shown; then cut segment off the dowel.



Use padded sanding block and drill press as an alternate method for sanding heads.

WORKSHOP MINICOURSE

GETTING AN EDGE

here's at least one time that splitting hairs can be a positive, happy experience. That's when you have the satisfaction of taking a dull plane blade and putting an edge so keen on it that it will, in fact, cut a human hair. Many fear that getting a good cutting edge is beyond their skills. In fact, it isn't.

For openers, you need a grinding wheel. Unless you plan to become a serious woodworker, you can do a beyond restoration honing alone.

To grind a blade, hold it at an angle against the grinding wheel and apply slight pressure as the wheel spins. Keep the iron moving back and forth in a straight line across the grinding-wheel edge. Besides reshaping the tool edge properly, you also want to make certain that the stone wears as evenly as possible.

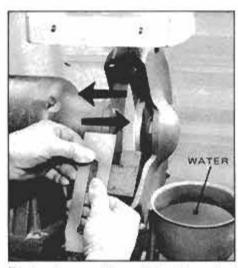
moving back and forth across the wheel, your goal should be to create ever-so-slight rounded corners, as

Once you're satisfied with grinding, you can move the iron to the oilstone. The oilstone, in fact, should be carried to any job away fom the shop in case a cutting tool needs a quick touch-up during a planing task.

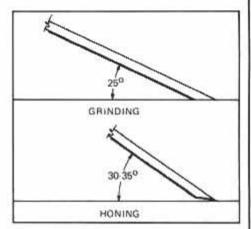
Put a couple of drops of light oil (oil (Please turn to page 150)



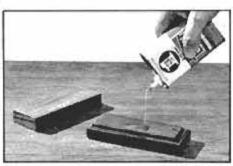
Grinding is done to straighten the edge of a cutting tool. It's done before sharpening by honing. The grindstone should turn toward the plane iron (arrow). The guide supporting the plane iron is set so the bevel will be ground accurately. The guide should always be used for that reason.



The iron is moved from side to side so that the full surface on the edge bevel is ground straight. This also keeps the wheel true.



The bevel angle for chisels and Irons might vary slightly, depending upon whether the tool will be used in hardwood or softwood. in general use, however, 25° is the acceptable bevel for grinding. Whetting is done at a 30° to 35° angle at the very edge.



A few drops of light oil applied to the stone serve as the vehicle for carrying steel particles away from the cutting edge. Use only quality oil and be sure to wipe the stone with a clean rag after each use.

great deal with one of the smaller, less expensive versions, such as Black & Decker's Model 7901 (\$39).

Don't run to your grinder every time a plane iron or chisel's cutting edge needs a touch-up. For the most part, you should master the art of whetting (honing) to keep almost surgical-tool quality edges on all your cutting tools.

Grinding should actually be done only if the edge is nicked or if it is



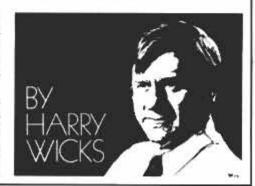
Whet the cutting edge on the oilstone to create the sharp edge. Hold the bevel edge flat against stone at 30° and work with a circular or figure-eight motion. The stone here is protected by a rosewood box the author made for it several years ago.

Have an open can of water nearby-my grinder is equipped with a tool-hung bucket-so that you can quench the tool to prevent it from becoming overheated with resultant loss of temper.

The edge should be ground so that it is almost at 90° to the sides. But professionals have known for years that a plane is easier to handle if you round the iron's corners ever so slightly. Thus, as you grind a bevel



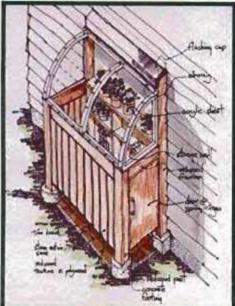
The feather edge—often called wire—is removed by placing the flat side of the iron on the stone and making a few strokes. Do not create even the slightest bevel on this side of the Iron, however.



Popular Mechanics

1932 OUTDOOR

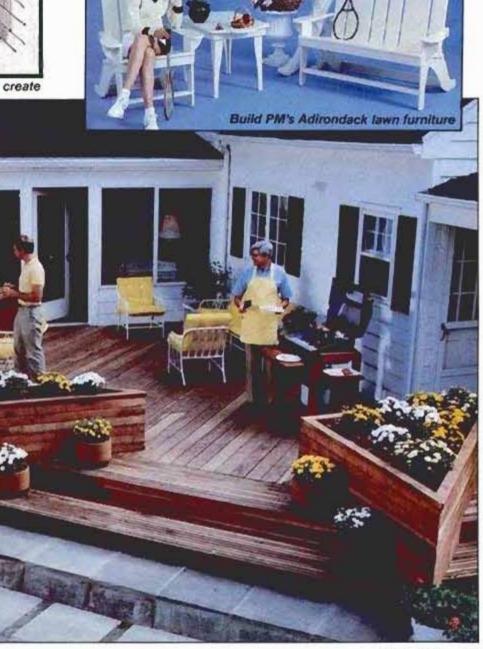
TIVING GUIDE



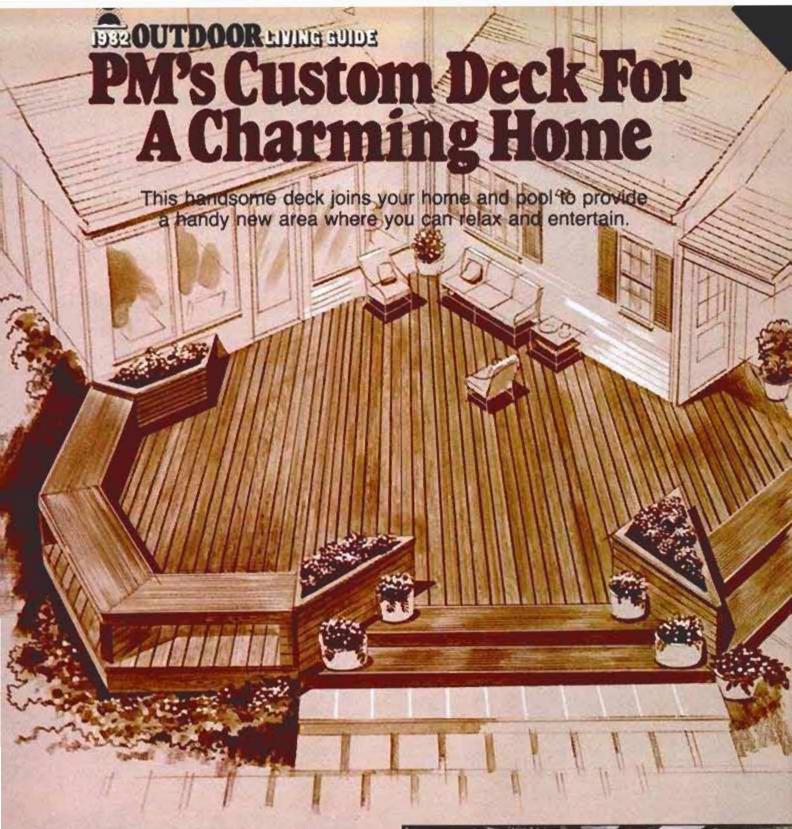
Yard storage structures you can create

Elegant pool deck goes with any house

It's true. You can find happiness right in your own back yard. Using your do-it-yourself skills and the Popular Mechanics projects on these pages, you can make your home the envy of the neighborhood.







of lawn leading to the swimming pool seemed tailor-made for the PM deck. The major access to the house from this deck is through an enclosed sun porch that adjoins the kitchen, making the deck a perfect area for outdoor dining. The deck also has a secondary access to the house, which helps the traffic flow and adds to the deck's appeal as a party spot. Another advantage this deck has for its owner: Situated between house walls, it provides a shady area much of the time, offering a retreat from the sunny pool side.

Ira Grandberg, A.I.A., designed this multiangled deck.

The benches and planters follow the deck angles and

The deck is constructed of knot-textured garden-grade redwood. We used construction heart (all heartwood) for

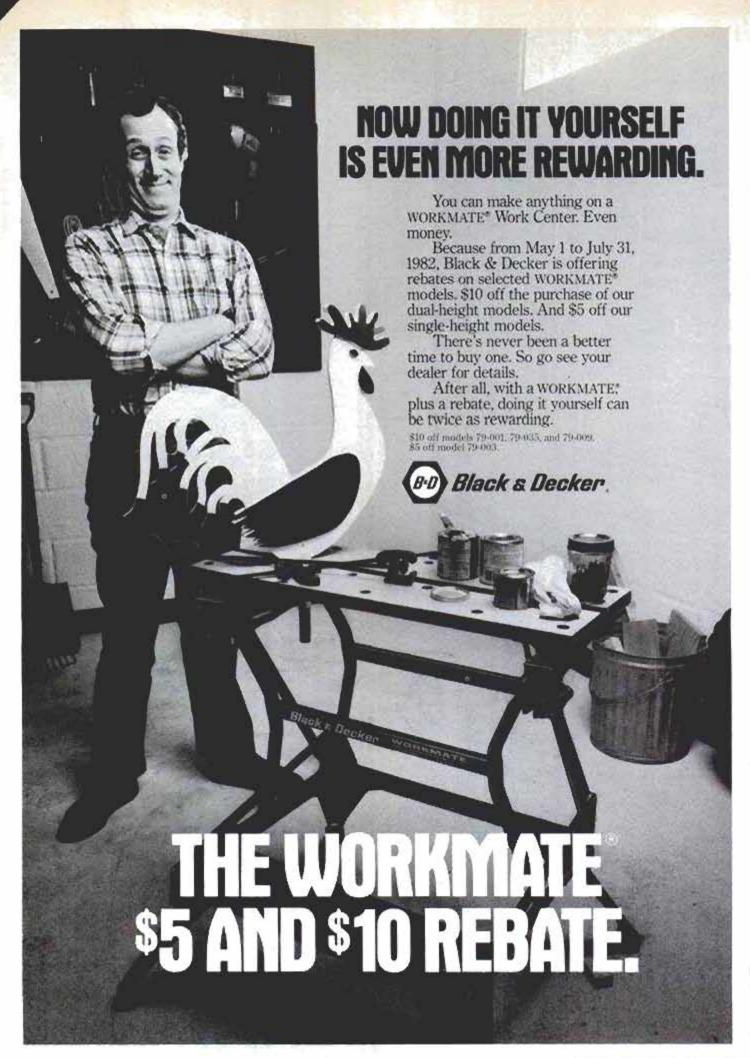
obviate the need for conventional railings.



View from across the pool shows how deck is designed to follow the contours of the house, and visually link it to the pool.







ESECUTDOOR LINES SUIDS

the posts, joists and near-ground applications. Construction common redwood, containing sapwood, makes up the decking, benches and trim.

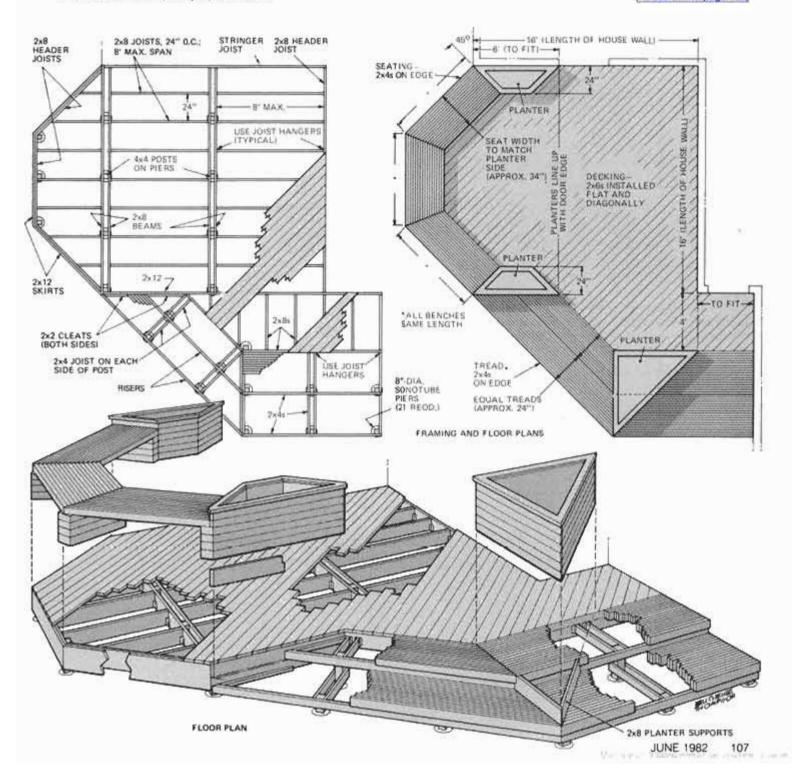
To begin work, lay out the deck area, using a mason's line and stakes, making sure the line is level. To prepare the site, use a spade to remove sod to a 2- to 3-in. depth. Prepare an area at least 2 ft. beyond the perimeter of the deck. You can landscape this later with bushes or with a ground cover, such as gravel or bark chips. Cover the below-deck site with black polyethylene sheet so weeds won't grow through your deck.

Design: Ira Grandberg, A.I.A.; Construction: Ross Tiefenthaler; Technical art: Eugene Thompson; Illustration: Dean Ellis; PM photos: George Ancona; Redwood: California Redwood Assn., 1 Lombard St., San Francisco, Calif. 94111; Barbecue: Model 824J Avanti with cart, Charmglow Products, Box 127, Bristol, Wis. 53104; Bug light: Model 8305, Charmglow Products; Furniture: dining furniture (Rondo style), lounge furniture (Bayshore style), Winston Furniture, Suite 908, Independence Plaza, 1 Independence Dr., Birmingham, Ala. 35259; Accessories: Leisure Unlimited Inc., 606 East Post Rd., Westport, Conn. 06880.

Locate and dig holes for the footings supporting the 4×4 posts. Footings must be below the area frostline to prevent the deck from heaving with the freezing and thawing of the earth. Footings should be spaced no more than 8 ft. apart. We poured concrete piers into Sonotube forms and used a step-flange anchor in each pier. Bore holes in the posts as needed to bolt them to the anchors. When you install the posts, make sure they are plumb in both planes, and in alignment with each other. Use a spirit level to check vertical alignment. While the posts are exposed, apply a water-repellent coating with mildewcide to them.

Next, 2×8 beams are fastened to the posts with ½ × 8-in. hot-dipped galvanized carriage bolts. (Note: Hot-dipped galvanized hardware and fasteners are used throughout.) When fastening boards on edge, sight along the edge to determine if there is a crown (convex edge); if so, install the board, crown edge up. The weight of the deck will tend to straighten the stock. A board installed convex edge down will sag after a while. (Boards installed on the flat should also be placed with the

(Please turn to page 108)



PEROUTDOOR LIVING GUIDE

(Continued from page 107)

convex surface on top for the same reason.) To calculate the attachment point of the beams, first determine the height you want the deck. This is based upon house floor elevation. The finished deck should be located 2 to 3 in. below the door's sill. Use a mason's line and a spirit level to determine the deck height; subtract the thickness of the deck board. The top of the ledger strips should be located at this elevation.

We fastened header and stringer joists to the house joists. To do it, remove the siding so the header and stringer joists can be installed flat against the sheathing. Snap a chalkline along both walls. Then run a hefty bead of caulking along the chalkline and install the joists with ½ × 6-in. lagscrews and washers. If you have a brick house, you must use expansion shields and bolts.

Before you begin fastening, make sure there's no chance of hitting in-the-wall electric or plumbing lines. Play it safe and make a trip to the basement to inspect the wall area. The deck joists are attached to these joists and to the beams with joist hangers. Joists in the area of the steps are of 2×4 stock, which accommodates 2×4 decking installed on edge above (see decking and step cross-sectional drawings). A 2×12 skirt is fastened to the outboard header joists to give a finished appearance.

Install the 2×6 deck boards, using 12d nails. To avoid splitting the wood, blunt the nail points slightly by striking them with



The built-in planters at the steps serve as railings while displaying blooms or greenery. Rather than being cantilevered over the step, both are supported from below (see the drawing).

a hammer. Place the nails no closer to the board edge than half the board thickness; no closer to the board end than board thickness. Set all nailheads slightly below the surface, using a large punch or heavy nailset.

There should be a 1/8- to 1/4-in. spacing between the boards to allow for water runoff. To add visual interest, the architect specified diagonally installed boards. To ensure a straight deck edge, trim off all the boards at one time after installation. Snap a chalkline at the trim mark and make the cut with a combination blade in a circular saw, its blade set so it just cuts through the decking.

Continuity of pattern

A band of spaced 2×4s on edge at the deck perimeter leads to a step of similar 2×4s on edge. This band continues the wood pattern of the seating. A flagstone step leads to a small flagstone area that provides an attractive transition between the wooden deck and the masonry pool walk. The flagstone is added after the planters and seating are installed.

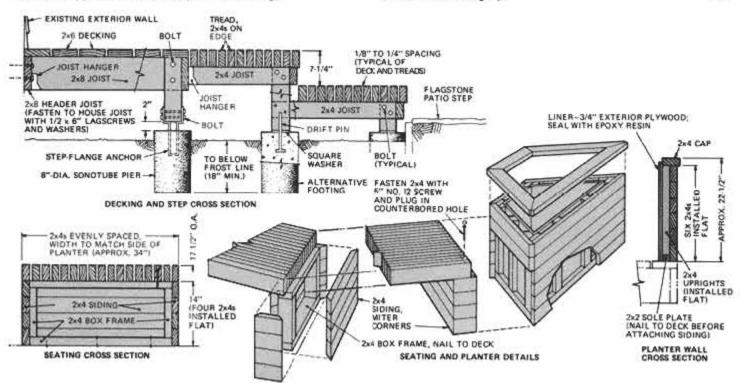
The bases of the seating units are simple 2×4 box frames nailed to the deck. The frame is sheathed with 2×4s installed on the flat, their ends meeting in miter joints. The seating surface is composed of 2×4s on edge, evenly spaced the length of the bases. The 2×4s are fastened to the bases with 5-in. No. 12 screws; the holes are counterbored and plugged.

The planters are similar in construction to the seating bases. Nail a 2×2 base for each planter to the deck. Then install 2×4 uprights on the base. The planters have a liner of ¾-in. exterior plywood sealed with epoxy resin. Install the 2×4 siding on the flat and top it with a 2×4 cap rail around the top. The two planters flanking the stairs are supported by 2×8s at the point where they overhang the step area.

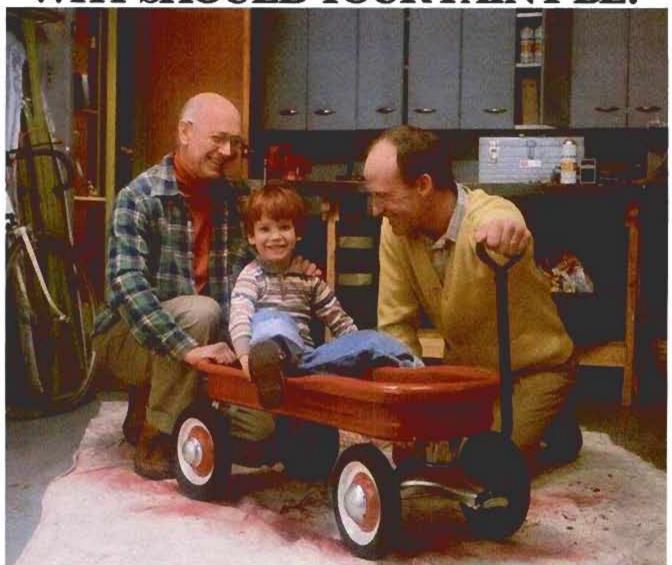
You can finish the exposed wood members shortly after completing the deck, using a clear water repellent containing a mildewcide, such as Woodlife, Cuprinol No. 20 or Pentaseal.

Before you treat the deck, be sure to remove any dirt with a detergent solution. After washing, rinse the wood thoroughly and let it dry for two or three days before treatment. Lightly sand off any lumber-grade stampings. To ensure an even application, spread the preservative over the wood with a pad applicator or a brush. For the best results, apply two light wetting coats, allowing one day's drying time between each coat.

To keep the deck in top condition, you should reapply the water repellent every two years. Gradually, the wood will weather to a driftwood gray.



IF WHAT YOU'RE PAINTING ISN'T ORDINARY, WHY SHOULD YOUR PAINT BE?



Your father gave you that wagon for your fifth birthday. Now it's your turn to pass it on.

Any spray paint could make it red again. If that's all you wanted.
But Rust-Oleum, with an average

of 50% more protective ingredients

than its nearest competitor, can do more. Its protective formula not only prevents rust, it also gives metal better protection from chipping, peeling and

RUST-OLEUM CORPORATION fading than other spray paints.

Something you might want to think about next time you're painting something you care very much about.

ANY METAL WORTH PAINTING, IS WORTH PROTECTING.

ST-OLEUM



Little Sheds For Your Yard

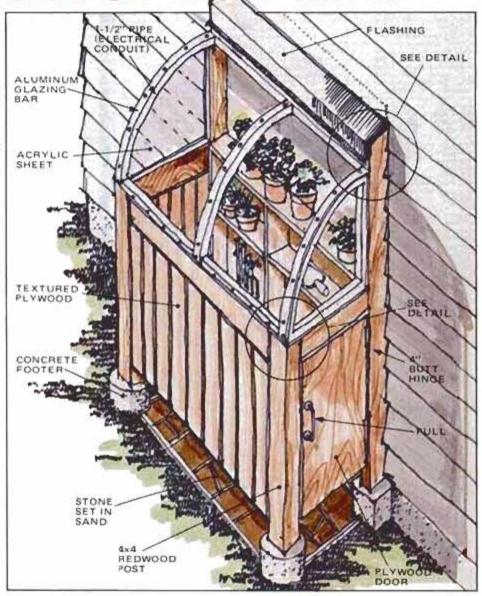
If your garage is already full and your back-yard space is too precious for a yard building, get the storage you need with these lean-to sheds. The designs, done for PM by Ira Grandberg, A.I.A., should inspire you to build your own with the dimensions that work best.

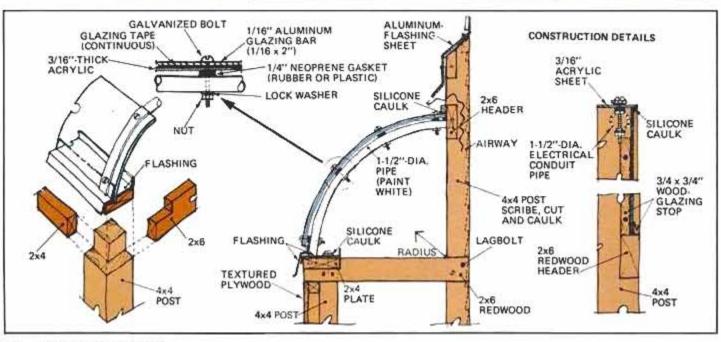
Garden Supply Shed

se the lower shelves for garden supplies and small tools, and the upper shelves for plants. The acrylic allows the sunshine in, but it protects plants from heavy frost. In the spring, you can make room in your hotbed for delicate garden plants by moving the hardier ones (pepper, cabbage, onion) into the cold frame at the top of this shed. Chrysanthemums and geraniums will continue to bloom in here long after autumn frosts have killed most other plants.

Remember that concrete footings must extend below the frostline in your area. Building suppliers carry a variety of ready-made concrete footing forms. Use 1/4-in. or 3/8-in.-dia. lagscrews 2 ft. on center to secure the shed frame to the house frame. Screws should be long enough to penetrate the siding and studs.

(Please turn to page 112)





POWER TO GO.









The Honda EM-500 portable generator is really going places. The mountains. The lake. The desert. Out to sea. Anyplace people need to power camping accessories. Charge their boat or RV batteries. Run small tools or appliances. Or shed some light on emergencies.

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THE STRONG, SILENT TYPE.

The EM-500's superb engineering design

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Whether you need power in some faraway place, or just want emergency auxiliary power in your home, you'd have to go a long way to beat the Honda EM-500.

To see one, stop by your nearby Honda Generator dealer.

For the one nearest you, just look in the Yellow Pages, or call toll-free 800-447-4400. And in Illinois 800-322-4400.

IT'S A HONDA

Trash Can Hideaway

ou may be enjoying your yard more than ever before, but you'll take even greater pride in the view if you can manage to hide away unsightly garbage cans.

This shed does the job without sacrificing accessibility to the cans. Regardless of the size of the cans you choose to keep in the shed, they can be easily removed through the doors below for trash disposal.

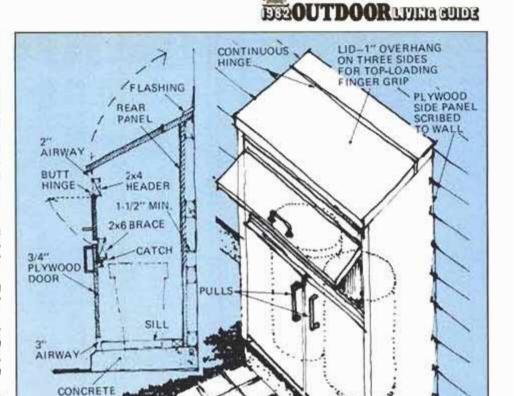
The upper doors allow for easy trash deposit, whether in large quantities (top door) or small scraps through the upper front door.

Pour a rectangular concrete slab, using plywood forms. Install the anchor bolts for the sill.

The "lean-to" design allows for construction without a frame because the house wall provides structural support. Note that the shed dimensions must be based on the size of your trash cans:

It's important that you remember to leave clearance for easy removal of the cans through the bottom doors.

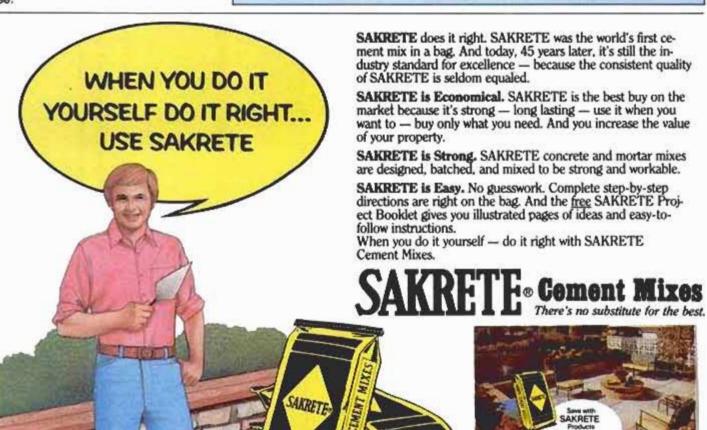
All of the sheds in this collection may be finished to match your house. Whether you use textured or smooth-surfaced plywood, you should make certain that the plywood you select is rated for outdoor use.



Ask for the SAKRETE Project Booklet — it's free at your SAKRETE dealer. Or send 75° for handling to: SAKRETE, Dept. P62, Box 17087, Cinti., OH 45217.

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PAV NG

STONES

SLAB

If you really want to enjoy your backyard, insist on



Electronic Bug Killers

Don't let mosquitos and other anti-social insects drive you away from your backyard fun .

Flowtron, the world's most popular electronic bug killer, takes care of those annoying insects so you can relax and enjoy the outdoors.

Flowtron's high-intensity black light lures flying pests to a self cleaning, electrically charged killing grid that disintegrates them on contact. No toxic chemicals or poisons - completely safe for people and pets.

The handsome, lanternstyled Flowtron bug killer operates for just pennies a day and is ruggedly built to last for years.

bug killer. Available now at leading stores in your area.

Enjoy your backyard again with Flowtron, the electronic

FLOWIR Two Main Street, Melrose, MA 02176

Tool Storage Shed

ccessibility is the most impressive feature of this shed. Miscellaneous small tools or the unwieldy garden hose can be stored in the upper bin, while heavy equipment can be rolled up the ramp and into the lower area.

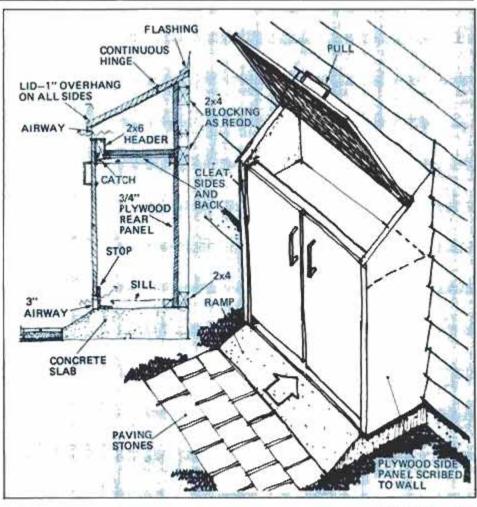
Decide on the dimensions that will suit your storage needs. Then excavate for the concrete slab and footings. Remember that footings must be below the area frostline.

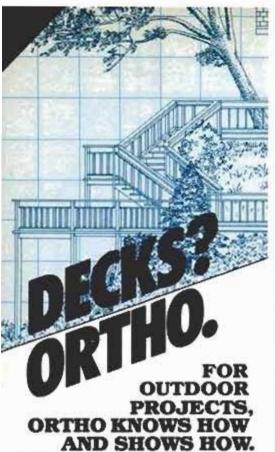
Notice that the ramp is part of the slab and is angled at about 25°. Construct the slab form of plywood and incorporate the ramp into the form. Secure forms with stakes and backfill soil outside between the stakes. This will provide added bracing for the form.

Before the concrete sets, bury the heads of three lagbolts (1/4-in.-dia. × 5 in.) along the sides of the slab for securing the sill. Leave the threaded end of the bolt extending up about 2 in. Make sure it is plumb.

This shed does not have a frame. The doors are hinged directly to the side panels and the panels are attached to headers and to the sills which are bolted to the slab. The floor of the upper bin provides rigidity for the structure.

(Please turn to page 114)





Build the yard of your dreams — it's easy, with ORTHO books. How to Design and Build Decks and Patios and Wood Projects for the Garden are full of imaginative ideas, full-color illustrations and easy-to-follow instructions for everything from planters and patio furniture to full-scale decks. With ORTHO knowhow and your handiwork, you'll get good-looking, money-saving results you can enjoy for years.

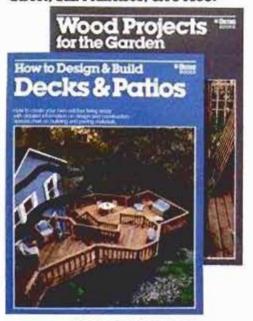
Look for the complete line of ORTHO how-to books at your nearest home and

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All-Purpose Shed



ong-handled garden tools such as rakes, hoes and edgers practically demand their own storage space. This shed is designed to keep those tools neatly and safely corralled. Its open-out-doors design keeps articles in full view.

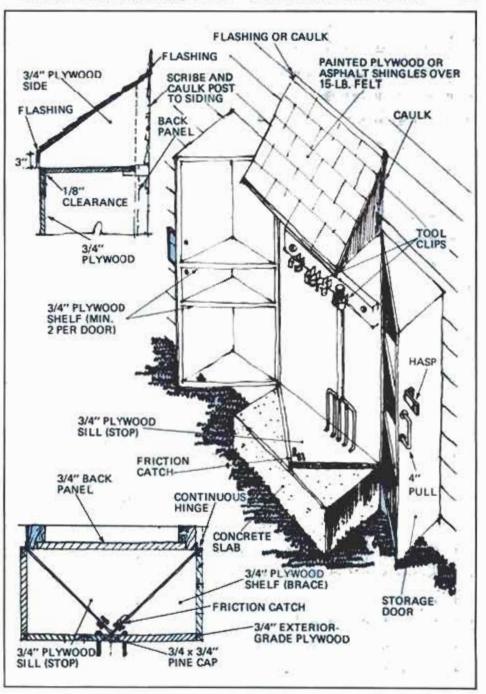
The shed sits on a concrete slab (like the garbage-can shed). Secured to the slab is a partial plywcod sill, in the form of a right triangle, which serves as a doorstop. The front and sides of the shed itself are actually the open-out doors. They are attached to the 4×4s which, in turn, are scribed and secured to the house. The continuous hinges run the full length of the doors and are rated for heavy-duty use. Position the hinges carefully to ensure a good fit when the doors are in the closed position.

Available in hardware stores, continu-

ous hinge (often called piano hinge) is usually available in 6- and 8-ft. lengths. Cut the hinge with a hacksaw to suit your needs. Use brass hinge to avoid rust stains on the wood.

Note that the right-angle joints in the doors are rabbeted. Take care when assembling the doors. If the doors are not assembled accurately, they won't come together for a good fit when they are finally attached to the shed.

Cover the shed roof with asphalt shingles. Since you have less than 30 sq. ft. to cover, you won't need more than one bundle of shingles. Install the shingles, using standard roofing techniques. If you plan to use any of the area underneath the shed roof, be sure to snip off all nails through the plywood with nippers, or clinch the nails with a hammer.



A TOP TO BOTTOM COMPARISON OF GAS POWERED TRIMMERS.

All controls and starter up at your fingertips, away from cutting head. For easier operation, easier starting, no bending over.

> Top-mounted engine keeps carburetor and air cleaner out of dust and flying debris, for less maintenance, longer life.

Top-mounted Weed Eater Trimmer

Engine is here, on top, for balance and easier handling, not at the end of a long shaft. So there's less fatigue and back strain.

Fuel is in this shaft, instead of a fuel tank.

Adjustable handle allows you to change cutting angle for edging. No way with / a bottom-mounted engine.

WEED EATER

Bottom-mounted competitor

Engine mounted down here, at the end of handle. Unwieldy and not well balanced. And right down in the dust and debris of cutting.

Almost impossible to change cutting angle for edging.

> Direct drive, with no clutch to stop cutting head from turning while starting and at idle.

Starter is down here, near the cutting head. So you either bend over or lift the who e machine. Awkward

Bulky engine obstructs cutting under low bushes and shrubs.

Sleek, streamlined and engineered to look like the high-quality marhine it is.

> Just tap the head on the ground for more cutting line. Clutch automatically keeps head from turning while starting and at idle.

WEED EATER, ON TOP FOR A LOT OF GOOD REASONS.

Build PM's Adirondack

Fifty years ago, lawn furniture like the handsome pieces shown here could be found on the front lawns of mansions and vacation spas. But now, you'll be able to make your own sets. The very look of this classic wooden lawn furniture conjures up images of more serene times. Though simple in design, this furniture from years past boasts an elegance all its own. Along with mission furniture, country pine and other older styles, Adirondack lawn furniture is currently enjoying renewed interest at antique shops and flea markets.

Though you may not have a sweeping lawn like those in front of the old mansions in New York's Adirondack mountain region—from which the furniture gets its name—PM's re-created set of lawn

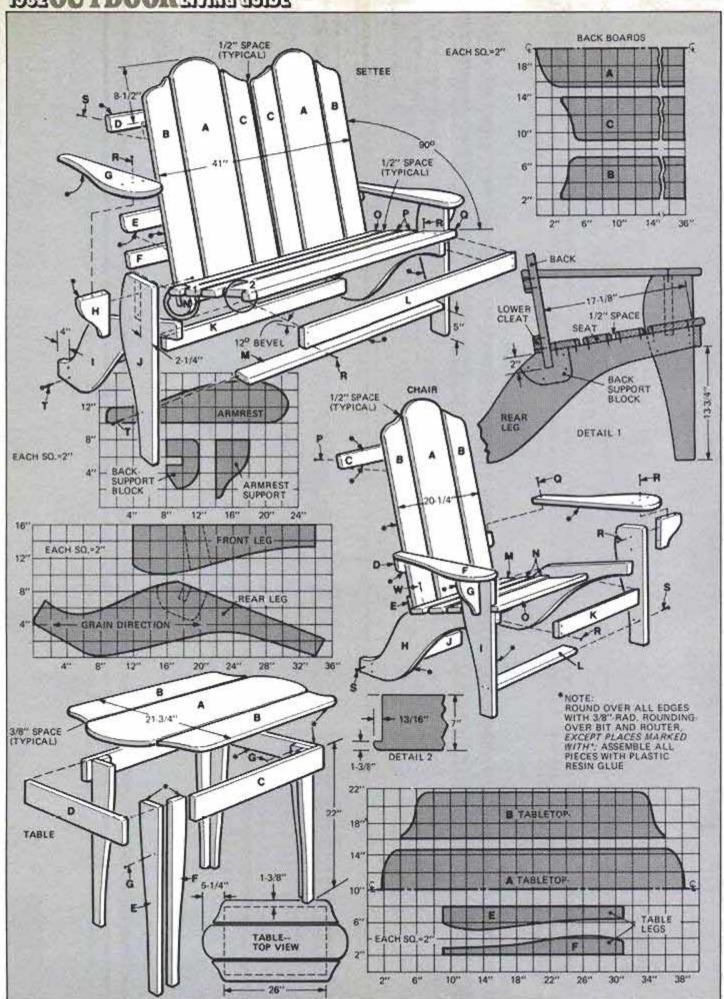


Lawn Furniture Set

Named after the beautiful mountains in upper New York State, this traditional Adirondack furniture is as durable as it is attractive. Crafted of hardwood, each piece is carefully sanded, then assembled with water-resistant glue, sealed with wood preservative and painted—all to make it stand the rigors of outdoor use.

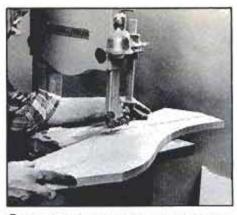
Design and construction: Rosario Capotosto PM color photo: Harry Hartman Technical art. Eugene P. Thompson

EGICE GLIVLIA GUIDE





1 If stock is dressed on the sides only, joint one edge, then rip to proper width.



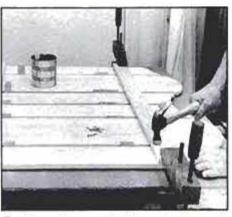
2 Use band saw or sabre saw to cut contours. Slow feed makes for better cut.



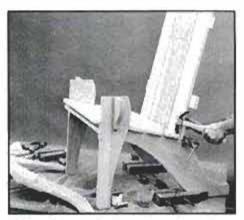
3 Use spoke shave to smooth saw marks. Always work with grain, not against it.



4 Start each assembly with legs. Use plastic resin glue throughout construction.



5 Clamp cleats to back boards, then nail.
Use spacers between boards for accuracy.



6 Bar clamp at back and spacing strip on front keep legs aligned while nailing seat.

MATERIALS LIST-SETTEE

Key	No.	Size and description (use)
A	2	13/16 × 91/4 × 36" poplar (back)
B	2	13/16 × 5 × 33/2" poplar (back)
B	2	1316 × 5 × 3312' poplar (back)
D	1	13/16 × 21/2 × 41" poplar (upper cleat)
E	1	13/16 × 21/2 × 441/2" poplar (middle
		cleat)
F	1	13/16 × 21/2 × 41" poplar (lower cleat)
G	2	13/16 × 5 × 22" poplar (armrest)
H	2	13/16 × 4 × 61/2" poplar (arm support)
1.	2	13/16 × 10 × 35" poplar (rear leg)
J	2	13/se × 6 × 22" poplar (front leg)
K	1	14h6 × 244 × 41° poplar (rear stretcher)
L	1	11/16 × 3 × 441/4" poplar (apron)
M	. 1	11/16 × 3 × 42% poplar (front stretcher)
N	2	13/16 × 4 × 6" popier (back support block)
OP	1	11/16 × 31/4 × 441/4" poplar (seat)
	2	11/16 × 31/2 × 441/4" poplar (seat)
0	1	11/16 × 7 × 441/4" poplar (seat)
B		6d hot-dipped galvanized finishing neils
S		4d hot-dipped galvanized nails
T	8	21/2" No. 10 th screws
*As re	qd.	

furniture is sure to evoke oohs and ahs when your neighbors spot it on your patio or deck.

Getting ready

For openers, carefully study the drawings and the materials lists. Do as professional cabinetmakers do. Make certain you understand exactly how the three pieces go together before ever putting a saw blade to a piece of wood.

You can use practically any kind of lumber to construct this furniture. If you apply wood preservative—an undercoat and two top coats of high-quality exterior paint—any species should weather quite well. We chose a hardwood, poplar, for its added strength.

The chair and table are made from 4/4 stock (13/16 in. thick), while the settee uses both 4/4 and 5/4 stock (11/16 in. thick). The poplar is easy to work highly warp-resistant and reasonably inexpensive. In the New York City area, it's only slightly more expensive than common pine and about half as expensive as clear pine.

Most hardwoods are sold surfaced on two sides only (S2S), not on the edges. So your first step is to cut the boards to approximate length, then dress one edge on the jointer. By keeping this straight edge against your table saw rip fence when ripping to the required width, you'll automatically dress the second edge.

Using the drawing grids for reference, make paper patterns for all contoured parts and trace them onto the appropriate-sized stock. The rear legs should be positioned so that the long part of the leg is in line with the grain direction of the board. When assembled, this will make the leg as strong as possible.

Next, cut the contours with either a

MATERIALS LIST-CHAIR

	. M	ATERIALS LIST—CHAIR
Key	No.	Size and description (use)
A	1	13/16 × 91/4 × 36" poplar (back)
В	2	13/16 × 5 × 331/2" poplar (back)
C		13/16 × 21/2 × 201/4" popiar (upper cleat)
D	1	13/16 × 21/2 × 233/4" poplar (middle cleat)
E	1	15/16 × 21/2 × 201/4" poplar (lower cleat)
F	2	13/1e × 5 × 22" poplar (armrest)
G	2 2 2	13ne × 4 × 61/2" poplar (arm support)
H	2	14he × 10 × 35" poplar (rear leg)
1	2	13ns × 6 × 22" poplar (front leg)
J	1	13hs × 234 × 201/4* poplar (reer stretcher)
K	1	13/16 × 3 × 231/2" poplar (apron)
Ľ	ţ	194e × 3 × 217/e" poplar (front stretcher)
м	1	13/16 × 31/4 × 231/2" poplar (seat)
N	2	13/16 × 31/2 × 231/2' poplar (seat)
0	2	14ha × 7 × 231/2" poplar (seat)
P	*	4d hot-dipped galvanized nalls
0		2" No. 10 fb screws
R	*	6d hot-dipped galvanized finishing nails
S	8	212' No. 10 th screws
*As re	gd.	

band saw or a sabre saw. Then sand out the saw marks from the edge with a drum sander mounted in your drill press or portable drill. Or, use a spoke shave. With a little practice, this easy-to-use hand tool will do a better and quicker job. Just be sure to use it as shown in the photo. Never scrape against the grain. Always stroke with the grain.

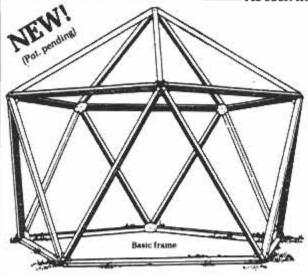
Next, round over all edges except those indicated with an asterisk (*) on the drawing, using a %-in. rounding-over bit in a router. This will give all the pieces a soft, comfortable look, while also reducing the possibility of wood splinters. All edges that have not been removed by the router

(Please turn to page 121)

MATERIALS LIST-TABLE

Key	No.	Size and description (use)
A	1	13/16 × 91/4 × 351/4" poplar (top)
В	2	*3/16 × 57/6 × 32" poplar (top)
C	2	*4're × 3 × 26" poplar (apron)
D	2	13/16 × 3 × 1731" poplar (apron)
E	4	13/16 × 3 × 22" poplar (leg)
F	4	1916 × 21/4 × 22" poplar (leg)
G		4d hot-dipped galvanized finishing nails
'As re	od.	N 30

As seen in Countryside magazine









Storage Building



Cerpori



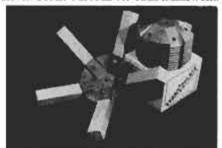
Build the frame in under two hours! Simply drill 50 holes and bolt your 2x4's to STAR*PLATES. Then, enclose it to make any small building

What small building would improve your property?

Build it better, easier, and cheaper, with STAR*PLATE Frame Connectors!

What are STAR *PLATES?

They're heavy gauge steel plates with channels to hold 2 x 2's, 2 x 3's or 2 x 4's, so they can be bolted together to make a basic geometric framework. It takes one set of eleven STAR*PLATES for each framework.



What size framework do they make?

Any size you want. STAR*PLATES hold 25 equal-length struts together. If the struts are short, say 2-3 feet, you've built something small like a dog house or small playhouse. If they're 9 feet long, you've built yourself a greenhouse or back-40 hideaway. The framework is made of 15 triangles, all exactly the same.

Is it hard to build?

Not at all. To build the frame, you only have to drill 50 holes, and bolt the struts to the STAR*PLATES with % in. carriage bolts. The only tools required are a saw, a drill, and a wrench. It's much simpler than conventional framing. For a roof and walls, or even a floor, our plans (included with each set) give you cutting patterns and tips to make it easy.

How can I finish it off?

Any way you want to. Some STAR*PLATE buildings are elaborate, with shingled roofs, clapboard sides, and fancy doors and windows. Others are merely covered with canvas and used as beach cabanas, or with plastic to make greenhouses. One cus-

tomer told us he was going to thatch his with palm leaves! You can make it as plain or as fancy as you want to.

Could I build a full-size geodesic dome house?

No. STAR*PLATES are designed for smaller structures. We recommend a maximum strut length of 9 feet That makes a structure 13 feet high and about 14 feet in diameter, with a floor area of 140 square feet.

How can I get a set of STAR*PLATES?

Order them from The Countryside General Store, Waterloo, Wisconsin 53594, for only \$39.95, postpaid. Phone orders are welcome (414/478-2139) and you can use your Visa or MasterCard.

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- 2. Beauty STAR*PLATE minidomes are unusual and attractive additions to any property.
- 3. Ease of construction no special skills or experience necessary.
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	today to: , 103 N. Monroe St., Waterloo, WI 53594

OUTDOOR भागिक क्यानि

(Continued from page 119)

should be eased with sandpaper before the parts are assembled.

Use highly water-resistant plastic resin glue for all assemblies. It is far less expensive than waterproof resorcinol glue and will serve quite well for this application. Also, hot-dipped galvanized nails are used throughout. Their dimensions are noted in the materials lists.

Because hardwood is being used, the nailing will be more difficult-a difficulty compounded by using the generally rough and nubby hot-dipped galvanized nails. For this reason, you may want to drill pilot holes for the nails. This will make driving them easier and will reduce the chance of splitting the wood.

In any case, do not nail closer than 1/4 in. to the edge of the board and % in. to the end of a board without using a pilot hole.

Assembling the furniture

Assemble the chair and settee in the following order: Glue and nail the back support blocks to the rear legs, then attach the rear legs to the front legs. Join the armrest support to the front legs, then assemble the backs as separate units. Use 1/2-in.-thick spacers to keep the back boards aligned properly.

Apply glue, then clamp and nail the cleats to the back boards, as shown in the

Keep in mind that the middle cleat is 33/4 in. longer than the backboard assembly is wide to accommodate the armrests. Just center it on the back. When the armrests are finally attached, the cleat ends can be cut to exactly the size and shape needed for a professional-

Next, support the leg assembly in the upright position as shown in photo No. 6, and slide the back assembly into the notched supports. Then attach the aprons, stretchers and armrests and you are done.

Assemble the table by first joining the aprons, then nailing the top boards in place. Nail the leg parts together. Then glue and nail the completed legs to the aprons from the inside.

To complete the job, sand all surfaces thoroughly and apply a high-quality wood preservative following the manufacturer's directions on the can. Then give all the pieces three coats of paint. PM

PLANS AVAILABLE

If you prefer to build this furniture from larger drawings, you can order a set of plans with instructions printed on 14×22-in. sheets. Send \$5.95 to Adirondack Furniture, Popular Mechanics, Box 1014, Radio City Station, New York, N.Y.



Insect-Aside" is a clean, silent version of the government-developed pest killer that can't harm pets, people or the ecology. Try it free for 15 days!

Here they come-the bugs of summer. The buzzing, biting, flying insect pests that drive you off your patio, spoil your outdoor barbecues . . . pool parties, cool-of-theevening hammock snoozes. But now you can enjoy bug-free comfort, thanks to new Insect-Aside **

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The heart of the Insect-Aside is a special type fluorescent lamp originally developed to clear swamps and protect cotton crops. Mosquitoes, gnats, moths, wasps, midges-almost 300 kinds of light-sensitive insects-are irresistibly attracted to it. And instantly, silently "zapped" by an electronic

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Since there are no poisonous liquids or sprays-no chemicals of any kind-the ecology isn't harmed in the slightest, Humans, pets, birds, even small infants, can't be hurt in any way.

It's so safe, many people use Insect-Aside as a nursery night light in summer to protect babies from mosquitoes. The only danger is to the buzzing, biting, irritating flying insect pests who flock toward its cool blue light and die there.

As a matter of fact, Insect-Aside™ won't even bother the useful insects-ladybugs, butterflies, bees-that you want around to help keep your garden and plants healthy.

U.L. LISTED FOR YOUR PROTECTION AND MADE OF SHOCKPROOF. RUSTPROOF MATERIALS

The simple, attractive lantern design stands or hangs anywhere you want protection, plugging into any AC outlet. The case is virtually indestructible plastic-the same used for phones and football helmets.

There are no moving parts. And unlike some imported models, the lamp for this American-made unit is easily replaceable. It's

Attractive, lantern-style Insect-Aside** 11" high and weighing 3 lbs., rids your yard or patio of mosquitoes and other harmful bugs without sprays, fogs or poisonous chemicals. U.L. listed.

rustproof, shockproof, and UL listed for use both outdoors and indoors.

TRY IT FOR 15 DAYS AT ABSOLUTELY NO RISK

Try Insect-Aside™ for 15 days. If it isn't everything we say, return it for a full refund. You are protected by a 1-year limited warranty of the unit (including lamp) as well as Shelburne's 26-year reputation for satisfied customers.

YOU CAN'T BUY BETTER PROTECTION FOR TWO OR THREE TIMES THE PRICE.

Insect-Aside™ costs just \$49.95 (plus \$3.95 shipping and handling.) It's special type fluorescent lamp uses about 7¢ worth of electricity a month, and lasts 6 to 7 years in notmal service. Yet it gives the same type of protection as units costing up to \$200-without their "snap, crackle, and pop."

ATTENTION CAMPERS AND BOATERS

Insect-AsideTM is available in a 12-volt model that plugs into any car, RV, or boat cigarette lighter. The Camper Model lures and kills insects from up to 1/2 acre for \$54.95 (plus \$3.95 shipping and handling). If you'd like to plug your Camper Model in AC current too, order it with the optional AC converter for \$66.95 plus \$4.50.

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DE OUTDOOR LIVER OUDE

Build This Garden Cart For The Big Jobs

This cart holds over eight bushels and stores tools and supplies.



Reduce the ends and round over the edges to make a comfortable grip. Screwfasten the bottom to the handles with ten 1½-in. No. 10 flathead screws. The handles should extend 1½-in. beyond the bot-

pilot screw holes.

steadily in popularity, because it hauls so much, so easily. These carts are expensive, but you needn't let the price keep you from owning one. For about half the price of a commercial cart, you can build your own cart with features that make it comparable to anything on the market.

Boxes above the wheels of the garden

he large garden cart is gaining

Boxes above the wheels of the garden cart shown here hold small tools and seed packets. Tool racks tote up to 10 lawn and garden tools. Removable gates at both ends quickly give you a 5½-ft.-long bed for oversize loads. And there's no need to bring sawhorses along for outside jobs: Just clamp work to the tool racks.

The design requires two 26-in.-diameter bicycle wheels with front hubs. You can adapt a rear wheel by removing the drive and brake mechanisms and making one of the boxes 1 in. wider. Old bikes are readily available at garage sales or bike shops. New bicycle wheels cost from \$25 to \$40 each.

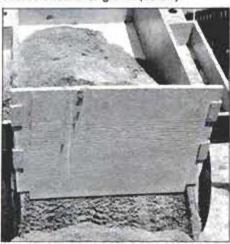
Begin by shaping the cart handles.

Next, attach the sides to the handles with six screws in each side. Notch the bottom corners of the end gates. Each gate is held in place by four lengths of angle iron (M). The upper two are screwed to the gate; the lower two are screwed to the sides so gate can slide up. Round over the gate edges slightly to fit the inside corner of the angle iron.

tom's front edge. Bore 1/e-in.-diameter

Attach the wheel angle irons and locate the mounting position by tipping the cart on end. A %-in.-thick scrap of wood under the gate will keep the cart steady. A ¼-in. piece under the wheel will give the right clearance. Be sure the 1½-in. No. 12 roundhead screws that secure the angle irons (N) are centered on the handle, rather than on the plywood side. Bore %4-in.-diameter pilot holes.

The lawn and garden cart carries long-handled tools in racks and small tools and supplies in boxes over the wheels (above). It can hold more than eight bushels of weeds and clippings (left). With the front gate lifted slightly, the cart can dump a 2-ft.-wide course of sand or gravel (below).



With the cart on a hard, level surface, clamp the legs to the handles so that the cart is level. Mark and trim the legs to length. Attach them to the handles about 1/4 in. from the rear gate with three 11/4-in. No. 10 flathead screws.

Glue and nail the storage boxes together as shown in the drawing. Screw-fasten the cleats to the box bottoms and attach them to the sides of the cart. Align all screw holes carefully and bore pilot holes when screwing into plywood edges.

Bore a 5/16-in.-diameter hole in the box bottom directly above the axle for the lagbolt that holds the strut (O). Flatten the bottom 2 in. of the strut. Align the wheels and mark the center of the axle hole on each strut. Punch and bore the holes. Adjust the wheel bearings and attach the

(Please turn to page 124)

DEPOUTDOOR LIVE OUDS

(Continued from page 123)

strut. It helps to hold it with a pipe wrench or pliers while tightening the lagbolt. Screw the tool racks to the sides, then screw the bottom pieces to the handles.

Finally, paint the cart with a heavy-duty enamel. And it's done.—John M. Nolte

MATERIALS LIST-GARDEN CART

Key	No.	Size and description (use)
A	2	34 × 31/2 × 72' oak, preferable (handles)
В	1	% × 23 × 48" plywood (bottom)
C	2	%s × 18 × 48" plywood (sides)
B C D	2	% × 18 × 241/4" plywood (end gates)
FG	2	44 × 31/2 × 161/2" oak, approx. (legs)
F	2	48 × 51/2 × 23' plywood (box sides)
G	2	% × 43/16 × 23" plywood (box bottoms)
HI	2	58 x 43/16 x 47/16" plywood (box ends)
H2	2	\$8 × 43ns × 47/ns" plywood (box ends)
1	2	34 × 34 × 23" oak or pine (cleats)
J	4	34 × 3 × 23" pak or pine (tool racks)
K	2	34 × 436 × 22" oak or pine (rack bottom)
L	2	26"-dia, bicycle wheels with axie nuts, washers, and lock washers
M	8	1/8 × 11/4 × 11/4 × 3" angle iron (gate brackets)
N	2	1/6 × 11/4 × 11/4 × 6" angle iron (wheel brackets)
0	2	'o'-i.d. × 141/o' galvanized water pipe (wheel struts)



To mount wheels, stand cart on its front end. Place a %-in.-thick wood scrap under front gate and a %-in. scrap under wheel. With angle iron attached to wheel, locate screw holes in center of oak handle.

P		11/4" No. 10 ft screws
0	16	5/8" No. 8 rh screws (to faster M)
R	4	11/2" No. 12 rh screws (to fasten N)
S		4d finishing nails
-	1.0	AND CONTRACTOR OF THE PROPERTY

T 2 % x × 21/2" lagbolts with 1/2" anchors (to fasten O)
U 4 % x washers

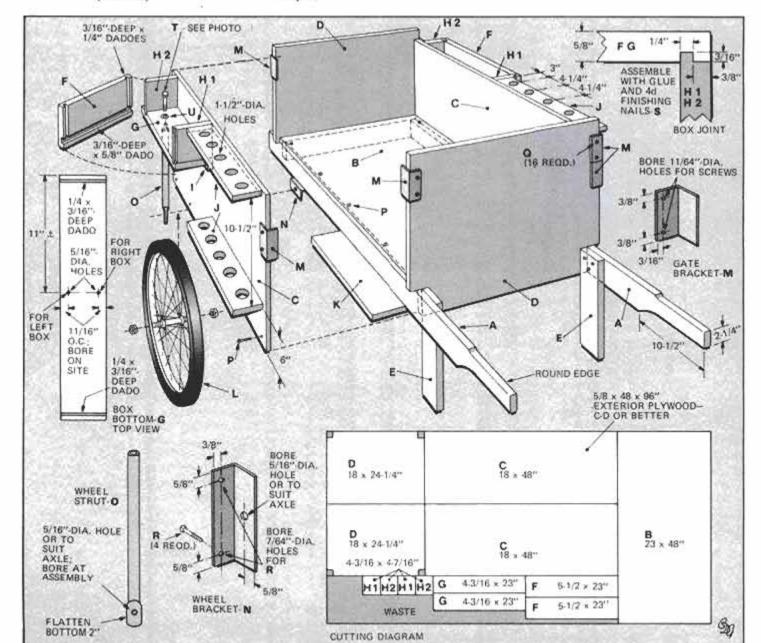
Misc.: Waterproof glue, primer and paint. "As required.



On a hard, level surface, clamp the legs of the cart to the handles so the cart is level. Double-check with a spirit level. Mark the legs and trim them to length.



Cutaway through a storage-box side and metal strut (O) shows how they're attached. A lagbolt with two washers passes through the box bottom into anchor in the strut.



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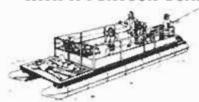
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DEZOUTDOOR LIVING GUIDE

Energy-Wise Guide To Spring Pool Maintenance

y taking these sleps when you open your pool, you can greatly reduce maintenance and operating costs:

1 Opening the pool

If your pool has been empty all winter, cleaning it is a three-step job: 1. Sweep down the coping, walls and bottom and remove debris. 2. Close the drain plugs and flush the piping; then drain water to the sewer. 3. Remove hard-water and



1 If the pool has wintered full, remove winter cover. The Lid, shown, by Heldor Industries, 1 Cory Rd., Morristown, N.J. 07960, is anchored by tubes of water.

rust stains with a commercial pool cleaner. Scrub organic stains with a rust stone or a stainless-steel brush. Remove grease from tile with mild detergent.

If the pool has wintered full, remove the winter pool cover, clean and store it. Close the drain valves on the filter and heater, flush, then drain the pipes.

2 Preparing the pump and filter

To ready the pump and filter, check the filter pressure gauge (or air valve) to be



2 Preparing the pump includes emptying the basket of debris and hosing it clean.

sure it is closed. Also make certain the pump is primed with water. Backwash the filter, then disassemble it and wash away caked diatomaceous earth.

For increased energy savings, set the pump timer to operate the pump a maximum of eight hours a day during heavy pool use and four hours a day during light use. Raise the filtration cycle time only if needed to control debris or algae. Also, run the pump during the less expensive energy-consumption hours.

3 Treating the pool water

The key to clear pool water is maintaining a balance between acidity and alkalinity, referred to as pH balance. Balanced



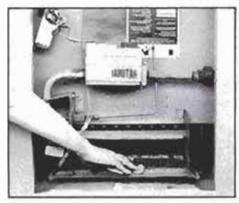
3 Kits, such as this Agua Chem, test water balance and help you maintain clear water.

water has a pH range of 7.2 to 7.8. Kits for testing the water's pH level are sold at pool-equipment suppliers. You should also test and adjust the water's total alkalinity, chlorine level and stability.

4 Heating the pool

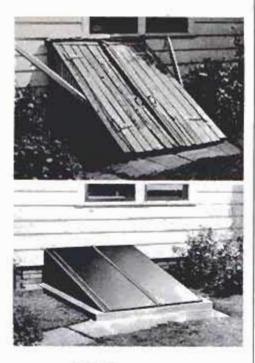
To prepare your heater, clean all open areas thoroughly. Set the thermostat; to assure that it remains at an energy-saving

(Please turn to page 128)



4 Before lighting the pilot light on the heater, remove debris at top and bottom.

Victoria Especialista de Media Parent



What a difference a Bilco Door makes!

Replacing that troublesome old wooden door with a modern Buco Basement Door will add new beauty and convenience to your home. You'll like its neat, trim appearance and rugged construction. Long lasting, watertight, safe and secure, it pays for itself in savings on repair and replacement costs. Ease of operation is a special BILCO feature. Torsion rods provide effortless opening and closing of the heavy gauge steel doors. A BILCO Basement Door is also the ideal addition to your present home or the home you're planning. It saves tracking through first floor rooms, makes the basement useful, convenient, safe. Just like adding a whole new floor of valuable living space.





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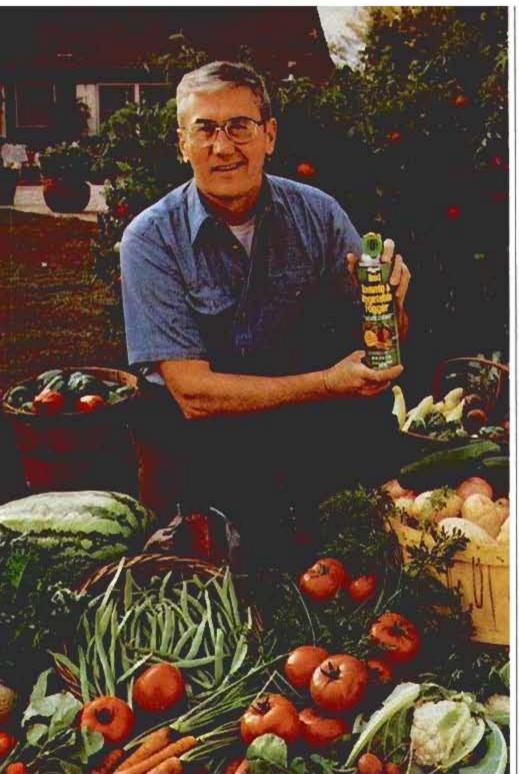
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"Look for it wherever Raid is sold."

Kills garden bugs dead. Naturally.







(Continued from page 126)

level (78° F. or below) use a setscrew. During the swimming season, operate the heater only when your pool is in use. It costs less to heat the water over a weekend than it does to maintain the temperature all week.

5 Using a solar pool cover

For added energy savings, cover your pool with a solar pool cover when no one is swimming. On the average, these covers raise the water temperature during the swimming season by 10° F, or more. They also impede chemical evaporation. Unroll the cover from storage and remove grime and algae with a soft brush and water.



5 Use a soft brush and water to remove grime and algae on the solar pool cover.

If your pool isn't used for long periods, remove and store the solar cover. This retards cover deterioration caused by exposure to ultraviolet rays and pool chemicals. Store the cover away from sunlight and in temperatures below 120° F. Note that a solar cover left on the lawn can kill the grass in an hour.

6 Solar heating systems

A solar heating system extends the swimming season and reduces energy use significantly. Cold pool water pours



6 If you heat pool water with solar panels, hose off accumulated leaves and grime.

through the sun-heated panels, where it is warmed and returned to the pool.

When you're opening your pool, hose accumulated debris off the panels. The panels shown (below) are made by Solar Industries, Manasquan, N.J. 08736.

The pool-opening procedures above were outlined by Jim Jones, a pool-care specialist. For a free booklet, Pool Care. A Complete Guide to Pool Maintenance, write: Jim Jones, Georgia-Pacific Corp., 1001 Dove St., Newport Beach, Calif. 92660.

Variation a substitution with the Prince

New Products For Outdoor Living

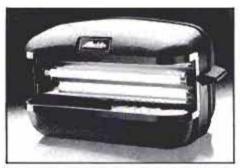


The Smoker-Grill (cutaway shown on left) is a combination outdoor cooker that lets you wet- or dry-smoke food; roast, bake and steam it; or just grill it by removing the upper smoke barrel, as shown below. It can

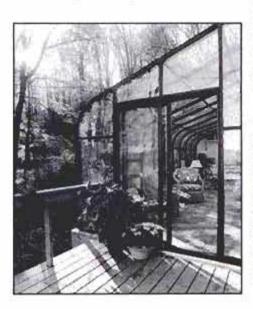
hold up to 50 pounds of food and comes in charcoal and gas-fueled models that cost between \$70 and \$110. At home centers, it's from Coleman Patio Products, 250 North St.

Francis, Wichita, Kan. 67201.





Aladdin's Bug Battler is a new insect killer that operates silently-no popping or zapping. Insects flock to 8-watt black lamp where an electric fan forces them into a water-filled tray below, where they drown. It's \$70 at home centers. From ALH Inc., Box 100255, Nashville, Tenn. 37210.



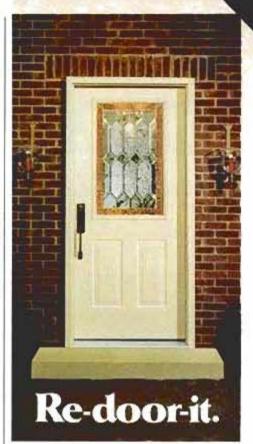


The Bagger Vac, a new attachment for the Ariens line of garden tractors, can vacuum leaves, grass and other debris from your yard. It includes a 12-bushel, rear-mounted container and an all-steel blower with ABS plastic hose. According to the manufacturer, the unit mounts in less than a half hour. It is driven by the power takeoff at the back of the tractor. The hose hooks over the discharge port on mower deck. The unit is about \$600 at dealers. It's made by Ariens Co., 655 West Ryan St., Brillion, Wis. 54110.

The Solarium is a new glass enclosure designed to be used as a year-round addition to your home. The modular glass sections are made of either clear or tinted insulated safety glass and aluminum frames, colored dark bronze or white. Each module is 30 in. wide, but the lengths differ depending on specific use. Made by Lord and Burnham, Box 255, Dept. 3060, Irvington, N.Y. 10533, the unit shown is about 10 × 25 ft. and costs between \$18,000 and \$19,000 installed. For information, write to the maker.

(Pease turn to page 130)

JUNE 1982



Now's a great time to replace your old entry door with a beautiful, secure, energy-efficient Therma-Tru door. It's pre-hung, so you can install it yourself with ordinary tools. Complete step-bystep directions are included.

Or, if you prefer, your Therma-Tru Dealer can recommend someone to help with installation.

Therma-Tru doors feature self-sealing sills and door bottoms that virtually eliminate air and moisture infiltration, arctic magnetic weather stripping that seals like a refrigerator door, and our patented adjustable security strike plate that wraps around the jamb to provide greater resis-

tance to forced entry. Choose from more than 100 beautiful raised panel or deeply embossed doors, and many leaded, beveled glass

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The new Waterbug™ At only 9 lbs., put lightweight, water pumping power wherever you want it.





At last, a high-performance, gas-powered pump for homeowners. The reliable, portable Waterbug™ Pump handles a wide range of water-moving chores with ease. Around your home. Around the pool. Dockside. Watering lawns or gardens. Dewatering basements. Washing outdoor surfaces. Emergencies. Whatever the job, Waterbug is a great new idea in pumps.

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For the name of your nearest Waterbug dealer, write to Homelite Waterbug, Dept. A82, P.O. Box 7047, Charlotte, N.C. 28217.

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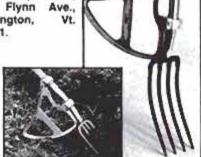
(Continued from page 129)



Weber's portable griddle for campers and picnickers is a heavy-duty, aluminum-alloy platter designed for use on its Smokey Joe Portable Table Top gas and charcoal grills. To use the 10% × 13%-in. unit, place it on the grill and let it heat up. At home centers for about \$20, it's from Weber-Stephen Products Co., 100 North Hickory Ave., Arlington Heights, III. 60004.

The Tiller-Aid is a cast-aluminum accessory designed to mount low on the handle of a spade or spading fork and increase the tool's efficiency. The maker claims it reduces the amount of effort required to operate the tool, thus relieving much of the back strain involved in spading work. It

holds up to 1½-in.diameter handles and is \$16.60 postpaid from Gardens for All, Dept. P83B, 180 Flynn Ave., Burlington, Vt. 05401





The Happy Cooker is a kettle-shaped barbeque grill with a five-way adjustable lid. The best model (shown here) has a 22-1/2-in. diameter and is made of steel with a red porcelain finish. With optional cutting board, it's available for \$90

at home centers. It's made by UNR Home Products, Box 429, Paris, III. 61944. PM

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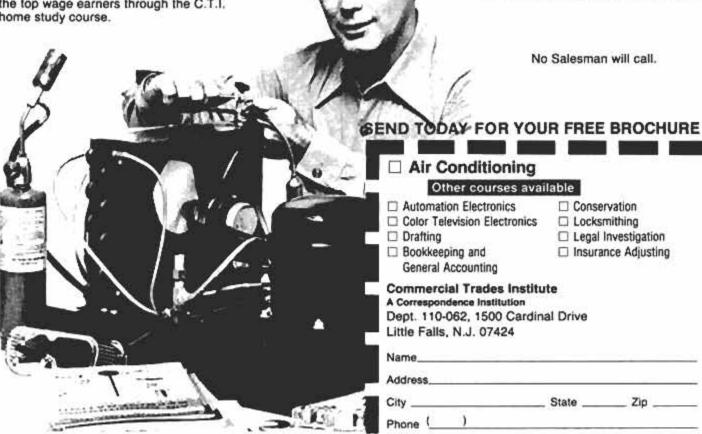
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If the coupon is missing write: C.T.I. Air Conditioning, Dept. 110-062 1500 Cardinal Dr., Little Falls, N.J. 07424



Build This Colonial Drop-Leaf Table

This handsome mahogany end table doubles as a serving table when its leaves are raised.

by G.S. Wing

colonial-style furniture never seems to lose its appeal. For people venturing through flea markets and roadside garage sales or for those frequenting antique shops and new furniture showrooms, the colonial piece is a magnet—and with good reason. Simple design and straightforward function are rarely

combined in a more elegant way.

This reproduction of an 18th-century drop-leaf table is a perfect example. Its solid mahogany construction is eye-catching and versatile. With leaves dropped, it measures less than 12 × 24 in.—just right for an end table. Yet, with leaves raised, the 23-in.-diameter

This reproduction 18th-century drop-leaf table is small enough to fit nearly anywhere.



top is large enough to be a serving table or a casual dining table for two.

Getting started

Before you begin, study the drawings on page 134. Because of its splayed legs, this piece is a bit more complicated than it appears.

Cut all parts to width, using the materials list on page 134 as a guide. Then turn the legs to match the shape shown in the drawing. Finishsand the legs while they're still in the lathe, using 1-in. strips of abrasive paper in a 100-, 180-, 220-grit progression.

Next, cut the 7½° bevel on the top of each leg and cut 7½° off both ends of the end aprons. Though we have supplied the length of the end stretchers in the materials list, you should take an actual measurement first. Do this by placing the legs and matching aprons on a flat surface and clamping them together lightly. Then take a measurement for the actual size of the end stretcher.

Next, cut the grooves in the aprons for the wood fasteners and bore dowel holes in all parts that will be joined.

I used a boring attachment on a radial-arm saw, but a drill press or a portable electric drill with a doweling jig would work as well. Remember to bore the holes perpendicular to the edge that's being joined. This ensures that mating parts will align. Note that both the stretchers and aprons join the legs 1/8 in. in from the sides of the legs.

Assembling the table

Bore the two pivot pin holes in the lateral stretchers 7½° off perpendicular, as shown. Make them ½2 in. in diameter—½2 in. bigger than the pins—to allow the support to move freely.

Test-assemble all parts to make sure everything fits, and glue and clamp the entire leg assembly. Check all lateral stretcher and leg joints for square.

Next, glue up stock for the tabletop and leaves into one 24 × 24-in. piece. Once dry, rip it into three boards—8, 8¾ and 8 in. wide, working from one side to the other. Keep track of which leaf was cut from which side, so when the table is assembled, the grain patterns will match. Now, cut the rule joint in all three parts.

Lay the boards flat on the workbench and place a 1/16-in.-thick spacer between each leaf and the top to simulate the joined position of these parts. Without disturbing the arrangement, draw a 23-in.-diameter

(Please turn to page 134)

MUST LIQUIDA

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Sure, you can buy "cheapie" models of "bargain basement" power tools at stores in town. But those tools are made specifically for cut-rate selling . . NOT for high quality! But with our huge liquidation of McGraw Edison tools, you get top-of-the-line quality at BELOW dealer cost! These tools are 100% U.S. made. Each one has a One Year Warranty. Although Shopsmith bought out McGraw Edison's power tool division, Shopsmith plans to continue producing these same great tools under their new Benchmark label.

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Powerful 8 arrp, motor for heavy-duty per-formance. Includes eye shields, end bells, adjustable tool rest, water trough, fine and coarse grif 6" x 3.4" grinding wheels. Has maximum motor speed of 3450 rpm.

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C. - 4" Belt Sander

1.6 hp. with standard 4" x 21%" belt size.
20 sq. in. pad size. Adjustable tracking mechanism. Sands flush to vertical surfaces.
Ball bearing construction. Reer handle and hand grip in front for maximum control.

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D. - 1/2" Variable Speed Drill

It's reversible, tool 1/3 hp, with variable speeds from 0-750 rpm, Lock button will maintain constant desired speed. Auxiliary handle included.

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E. - 3/8" Drill Press

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Also available in heavier duty 1/2" chuck with 1/2 hp. motor for only \$15 more.

F. - 7" Wood Lathe

Max. of 1/3 hp. Large calibrated control switch for variable speed control of 700-1800 rpm. Reinforced aluminum bed. To-tally enclosed motor. Adjustable tail stock for drilling, boring.

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Jefferson City, Mo. - McGraw Edison, manufacturers of products for consumer and industrial use, sells its power tool division to Shopsmith, makers of the famous 5-in-1 home powershop. Shopsmith acquires the remaining inventories of McGraw Edison tools and expects to continue producing the same tools under Shopsmith's own Benchmark label.

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COLONIAL DROP-LEAF TABLE

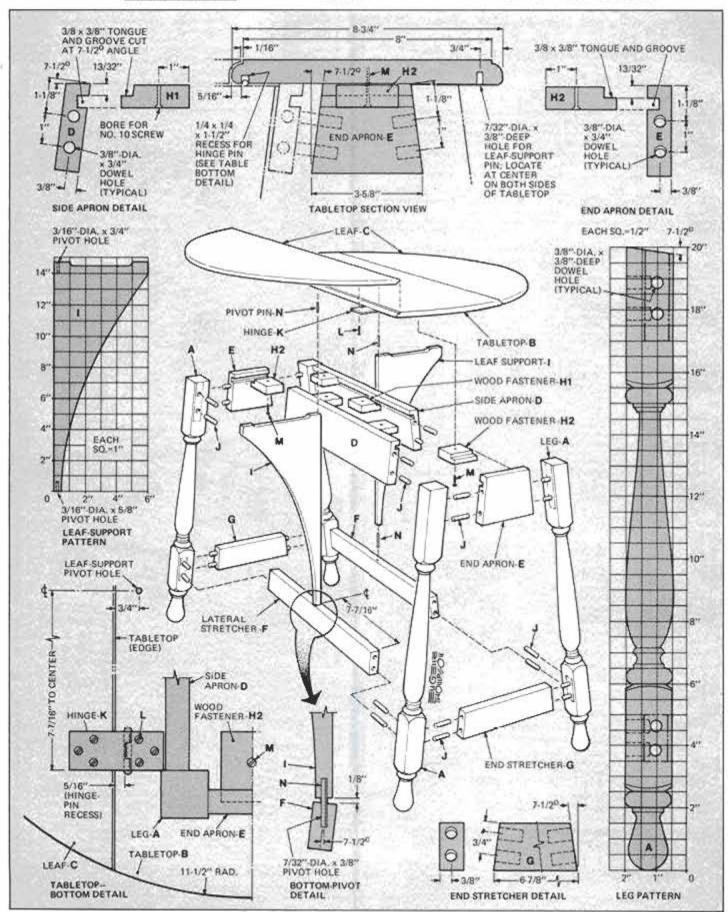
(Continued from page 132)

circle on these boards and cut out the shape. Round over the outside edges on both leaves and top—not the rule joint—with a router and a

(Please turn to page 136)

MATERIALS LIST-COLONIAL TABLE

		MAILTINES EIGI	OOL	211117L	INDEE
Key	No.	Size and description (use) 1-1/2 × 1-1/2 × 20" mahogany (leg)	G	2	3/4 x 1-1/2 x 6-7/8" mahogany (end stretcher)
В	1	34 × 8-34 × 23" mahogany (tabletop)	H:	6	44 × 2 × 2-1/4" mahogany (fasteners)
C	2	N4 × 7-1/2 × 23" mahogany (leaf)	1	2	12 × 5 × 14-34" mahogany (leaf support)
D	2	3/4 × 2-7/6 × '4-7/6' mahogany (side	J	32	46'-dia. × 1-1/2' dowel
		apron)	K	4	3" drop-leaf hinge
E	2	% × 2-7/e × 3-5/e" mahogany (end apron)	L	24	12" No. 8 fh screw
F	2	34 × 1-1/2 × '4-7a' mahogany (lateral	M	6	1-1/4" No. 10 fh screw
		stretcher)	N.	4	416'-dia. × 1-1/2' steel pin





Control anything electronic in your home with a preprogrammed command, or the touch of a button. Plus save on your heating and air conditioning bills.

Hats off to BSR. Their X10 remote control system started a whole new technology. Now from Regency comes the Regulator that gives you the ultimate control of your home.

Now you can remote control up to 100 lights and appliances, even those now controlled by a BSR system,

Plus, you can preprogram up to 16 daily commands to any of the controlled lights or appliances with the automatic quartz timing computer. You can let the Regulator take away daily drudgery or use it for security

???HOW IS IT DIFFERENT???

The BSR system really started the field of remote control lighting. So, how is the Regulator different?

Number One Difference. The Regulator is an intelligent system, While you can manually turn on and off 16 things with the BSR and 100 with the Regulator, only the Regulator can also execute 16 daily preprogrammed commands without buying extra timing units.

You can have your outside lights come on 30 minutes before you are due home from work, Your heat or air can come on an hour before you return so that

your home is comfortable.

Great Security-If you are away, you can have lights, radios, and TVs come on and off to give your home a lived-in look. You can even have lights come on

and dim if you wish to save electricity, Number Two Difference. The Regulator tells you what it's doing. It will display 'all on', or number 3 on 'dim', or whatever it is doing. Also there is a review feature that lets you review all preprogrammed commands. And, a standby feature that lets you leave preprogrammed functions 'on hold'.

Number Three Difference. The Regulator has a built-in three level automatic dimming circuit. You can select from the exact dimming levels of 55%, 30%, and 5% of any lamp in the system.

Number Four Difference. The Regula-

tor has a heating and air conditioning module you can add to let you control the times your heat will come on. You can limit the heat at night and while you are at work during the day. But, you can still have the house at the temperature you want when you get up in the morning or come home at night.
SIMPLE INSTALLATION

Really there is no installation required. Simply plug the Regulator into any AC plug. Then plug in an inexpensive UL approved control module at any light or appliance you'd like to control.

The Regulator sends out commands to its modules through your own house wiring. So, there's never any wire to run. Plus your system is just yours, because there are 100 programmable house codes to choose from.

EASY TO PROGRAM

Each module is as easy to program as turning stations on your TV. Each module has a dial, numbered from 00 to 99. Simply set any number. When you push that number on the control console, the light or appliance will go into action.

LOCAL CONTROL

Even though a light or appliance is programmed into the system, you can still turn it on and off with its switch.

It is manufactured and warrantied by Regency, the Radio Scanner People.
1/2 PRICE MODULES

We are offering the two most used modules at exactly 1/2 Price.

1) Lamp Module -- This is the module to use for controlling lamps pluged into the wall. It has 300 watt capacity, 100 codes and it has automatic 3 level dim. Retail \$21.95, DAK sold it at \$15.95, Now \$10.96. (\$1 P&H) Or. No. 9311.

2) Appliance module—This is the module to use for controlling appliances such as coffee pots, TVs, Stereos, and any home machinery. It has a 15 amp or 1/3 HP rating, 100 codes, Retail Price \$24.95, DAK sold it at \$17.95. Now just \$12.47 Order Number 9312.

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With two free modules valued at \$46.90

Regency has a problem. They built thousands of these computerized remote control systems they call the Regency Regulator to compete with the BSR X10 system. They built theirs to be much better.

The Regulator can handle up to 100 modules, the

BSR only 16. The Regulator has 3 preprogrammed dimming levels you set and a quartz timing system that automatically directs 16 programmed functions per day. Unfortunately, the Regulator was priced higher than the BSR and competition is rough,

So through a one time large cash purchase, DAK has 1500 master command consoles on sale for the one

time sale price of just \$69.90 (\$6 P&H) Or. No. 9313. EXTRA HOME BONUS—There's more. You get absolutely free one appliance and one lamp module to help you get started. Plus you can buy more modules with your order for the command console for 1/2 price. The Regulator is warrantied by Regency, and is compatible with BSR modules. Offer Limited.

SPECIAL MODULES

3) Heat and Air. This unit controls standard 24 volt thermostat wiring so that you can preprogram heat and air. This unit needs to be wired in by an electrician or yourself. Instructions included. Save on your energy bills. 100 code choices. Just \$49.95 (\$1 P&H) Order Number 9314.

4) If you want to control a light that is controlled now by a light switch, such as patio lights or room lights, we have the basic BSR light switch. It only has 16 code choices, but other than that limitation it works perfectly with this system including the 3 preset dimming levels, 500 watt rating. Just \$19.95 (\$1 P&H) Order Number 9217.
TRY THE REGULATOR RISK FREE

It's the professional system. It's built like a tank and it should last for years. If you already have a BSR system, it will be fully compatible.

Try the Regulator in your own home or office. If you aren't 100% satisfied simply return it in its original box within 30 days for a courteous refund.

To order your Regency Programmable Regulator risk free with your credit card call our toll free hotline, or send your check not for the \$149 list price, and not for DAK's \$119 old price, but send only \$69.90 plus \$6 for postage and handling to DAK. Order Number 9313. CA res add 6% sales tax, Remember you also get 1 free appliance module and 1 free lamp module. It's an extra value of \$46.90. Neither Regency nor DAK ever included a free module before.



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Send check, money order or use American Express, Visa or MasterCard, including all numbers and signature. (Add 6% sales tax for TN delivery.) For a color catalog full of old Tennessee items and Jack Daniel's memorabilia, send \$1.00 to the above address. Tennessee residents call 615-759-7184



shim leg to proper height and clamp to fence. Use same setup to bore leg holes.

COLONIAL DROP-LEAF TABLE

(Continued from page 134)

rounding-over bit, or use a molding cutter of your choice.

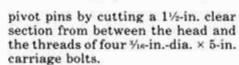
Once again, lay the tabletop, leaves and spacers finished side down on the workbench. Then center the leg assembly on top of it. Position the hinges as shown in the drawing and trace their outline and screw-hole positions. Chisel out the groove for the hinge knuckle and bore the pilot holes for the screws.

With the leg assembly still in place, cut the wood fasteners to size and position them. Bore the pilot holes for their mounting screws, but don't install the blocks yet.

Precision measurements

Measure the distance between the inside top edge of the lateral stretcher and the underside of the tabletop. Do this for both sides of the table, then subtract 1/4 in, from each to allow for free movement of the support. The drawing gives a detailed view of this construction.

Next, cut the leaf supports from 1/2in.-thick stock and bore a 1/4-in.diameter pivot pin hole in the top and bottom edge of both. (The bottom hole is 1/4 in, deep; the top hole is 1 in. deep.) Then fashion the steel

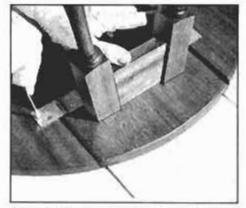


Clamp end aprons to table and fence; bore

dowel holes. Adjust boring attachment so

holes are bored perpendicular to edge.

Using the drawing as a guide, bore the two 1/2-in.-deep pivot holes in the underside of the tabletop. Each is positioned about 1° out of plumb in relation to the hole on the lateral stretcher. This deflection keeps the hole far enough from the edge so the rule joint isn't weakened. It also raises the outside edge of the sup-



Align leaves with tabletop, using spacers; then position and install each hinge, using aprons and legs as reference points.

port slightly, thus ensuring a tighter fit when pushed under the leaf.

Finishing up

Install the support, then attach the tabletop with the fasteners. Set the table upright and test the movement of the supports. If they're too tight, remove the top and sand the upper edge. If they're too loose, glue a small wood block on top of each support. Once the assembly is complete, sand the piece in a 100-, 180-, 220-grit progression and finish with a rubbing of 4/0 steel wool. Because Phillipine mahogany grain is so open, fill the pores with paste wood filler. When it's dry, sand it smooth, wipe with a tack cloth and apply the finish of your choice. I used paste furniture wax.





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Transfer leaf-support shape from grid on drawing to stock. Cut out with sabre saw or band saw, using a fine-toothed blade.

HINTS FROM READERS

Neater barbecue cooking



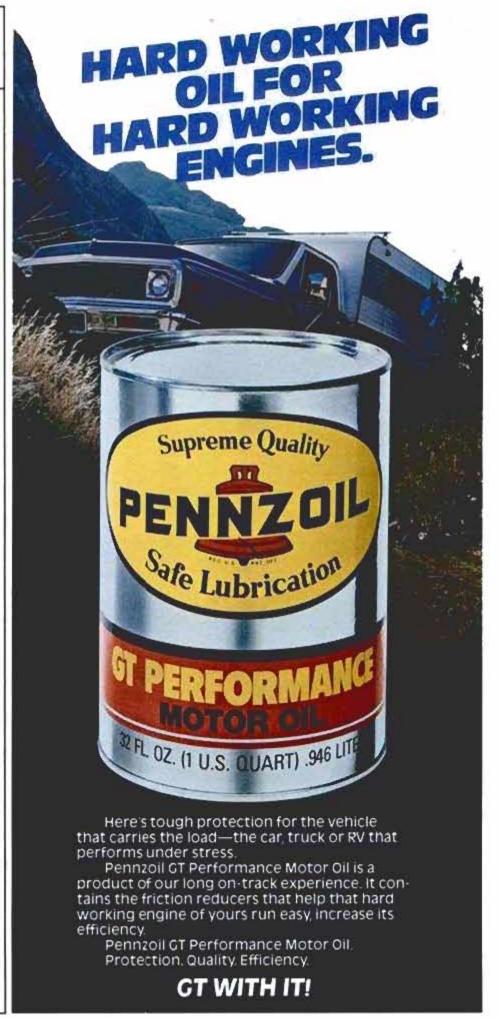
Although food tastes great after it has sizzled over charcoal, cooking can be hazardous when grease hits the coals, flames up and spatters. However, you can soak up greasy drippings and stop flare-up in the grill by putting a layer of cat-box filler beneath the coals. The filler absorbs grease as it drops. You'll also find that you don't have to use as much charcoal.—Susan Parker

Quick freezer defrost

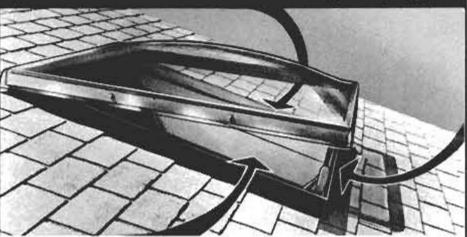


If your food freezer or the freezer section of your refrigerator has to be defrosted manually, do the job before the frost builds up. A heavy frost deposit on the plates or coils reduces the cooling efficiency of the refrigerator.

You can speed your defrosting job by using a portable hair dryer to blow warm air on the freezer surface. Note: Make sure you unplug the refrigerator before you begin defrosting. If your vacuum cleaner works as a blower, it will serve nearly as well as a dryer, even though the air it exhausts is at room temperature. Use a sponge and basin to pick up the water. Warning: Never use a knife or other sharp metal instrument to scrape the coils or plates. You may puncture them. A plastic windshield scraper or plastic pot and pan scraper both do good jobs.-Ralph S. Wilkes



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Build This Tomato Trellis

An easy-to-build tomato trellis like the one shown has several advantages over conventional, single support stakes. To begin with, one 8-ft. rack accommodates more plants—in this case, six—without sacrificing exposure to the sun. It also ensures air space between low-growing tomatoes and the ground. This cuts loss due to rot and insects.

Additionally, the trellis can support cheesecloth or similar material to shade newly transplanted seedlings or protect them against a late frost.

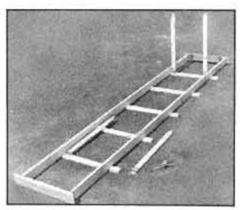
The entire rack is made of 1×2-in. furring held together with 6d (1½-in.) galvanized nails. The side rails are 8 ft. long and the cross-supports and hinged stakes are 22 in. long. But these dimensions can vary, depending on the species of tomato you're growing.

I found that 11/2-in. utility hinges worked well for joining the stakes to the cross-supports. And they facilitate easy winter storage: Just fold up the rack and hang it on a garage or basement wall.

These supports can last for many seasons if you coat them first with a wood preservative and two coats of exterior paint.—Wilson G. Walters



Trellis keeps fruit off the ground, thus holds crop loss, caused by rot, to a minimum.



Stakes are hinged to cross-supports for compact winter storage of complete trellis.

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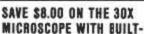
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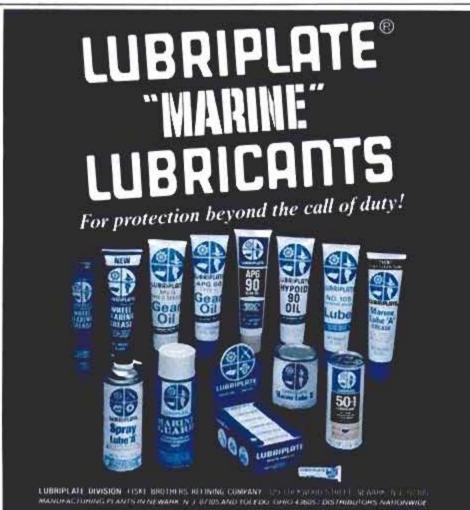
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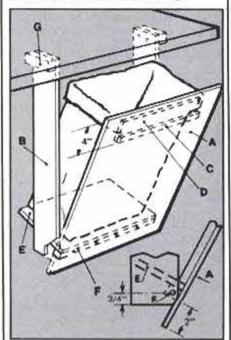
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HINTS FROM READERS

Quick workbench cleanup



The handiest addition to my workbench is a supermarket bag fastened to the bench with a few scraps of wood. This lets me pivot the bag forward to receive debris I sweep into it. The top edge of the bag turns down 1½ in. to form a lip, which hooks onto a bag support fastened to a plywood front.

Cut the parts to size. Locate the front so it fits under the benchtop with a 1-in. space at top. Prebore and fasten cleat C and bag support D across the front, 4 in. from the top edge, with 1/4-in. No. 4 screws. Prebore and secure platform E to cleat F with four 1/4-in. No. 6 screws. Prebore and attach cleat F to the front from the outside with four 1/4-in. No. 6 screws. Prebore and fasten verticals B to cleats G with 1-in. No. 8 screws.

Two 1½-in. No. 10 screws with four washers serve as pivots to open the front. Bore holes in vertical B and cleat F at pivot points and insert screws and washers. Attach G to benchtop with 1½-in. No. 10 screws.—William Lohrand

MATERIALS LIST-WORKBENCH BAG

Key	No.	Size and description (use)
A	1	1/4 × 14 × 20" plywood (front)
B	2	1/4 × 3 × 1912 plywood (verticals)
C	1	1/4 × 1 × 11" plywood (cleat)
D	1	1/e × 21/2 × 11" hardboard (bag support
E	1	12 × 712 × 12' plywood (platform)

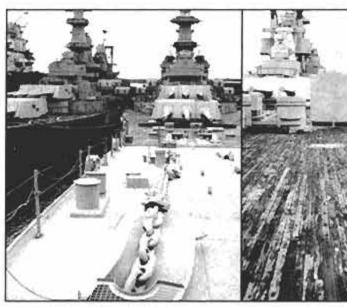
E 1 ½ × 7½ × 12° plywood (platform)
F 1 ¾ × 1 × 12¾ maple or other hardwood

G 2 % × 1 × 3" pine (cleats)

Misc.: Four 5t-in, No. 4 flathead screws; eight 1 ¼-in, No.

6 flathead screws; four 1-in, No. 8 flathead screws; six

1 ½-in, No. 10 flathead screws with washers (4).



Foredeck of lowa with Wisconsin lying alongside at Philadelphia Navy Yard. Dehumidifiers keep ships' interiors sound.

Jowa's teak afterdeck (rain-spotted in photo). Wood will be removed for landing pad and to patch other teak deck areas.

ing tonnage of capital ships were the crucibles of the world powers' foreign policies in the two decades leading up to World War II. Things began to fall apart in the '30s: Germany blatantly violated the Treaty of Versailles, building the battlecruisers Scharnhorst and Gneisenau and the battleships Bismarck and Tirpitz at tens of thousands of displacement tons in excess of specified limits. Japan had begun planning the huge Yamato-class battleships in secret in 1934, violating the Washington Treaty, and continued to maintain secrecy as two ships were built.

The Japanese were after an edge in time, maybe two or three years. In addition, they believed that even if the United States got wind of what the Yamato was all about, an appropriate response in dreadnought terms would probably have to be built in an East Coast yard and would be too large to fit through the Panama Canal. The Japanese failure to immediately ratify the London Naval Treaty of 1936 was duly noted by the Roosevelt administration; design work began in earnest



A 40-year-old geared analog computer aims the big guns. Electronics can't perform the job any better or more reliably.

in 1937 on what would be the lowaclass battleships.

They would be fast, capable of 33 knots flank speed, but with a slowcruising range that would take them three-fourths of the way around the world without refueling. Nine 16inch guns would each throw a 2,700pound projectile over 20 miles with accuracy. A full salvo would literally flatten everything within one square mile. They would be 887 feet long and, yes, they would fit through the 110-foot-wide Panama Canal . . . with slightly less than two feet to spare. Yet, one consideration made the lowas less than the dreadnoughts of their time: armor.

Even though the Roosevelt War Department suspected that the Japanese might field 18-inch guns (which they eventually did-the largest ever sent to sea), the main armor "belt" on the lowa-class was held to 12 inches thick at its maximum to cut weight in favor of speed. It meant that a direct encounter with Yamato-class ships might not be survivable. However, even before war broke out, most Naval strategists had moved well away from the idea that one dreadnought alone could be committed to attack another. There was too much to lose on either side. Even so, in the anxious months before Pearl Harbor, an almost ludicrous-considered in retrospect-series of false-intelligence figures was leaked from opposite sides of the Pacific: While we overstated our armor dimensions, the Japanese deflated their gun-barrel diameters.

What could be more unlikely than (Please turn to page 142)

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BORN-AGAIN BATTLEWAGONS

(Continued from page 141)

to find, 40 years later, an entire new generation of military men forced to issue daily assurances from the Pentagon regarding the no-sweat survivability of those same ships?

This time, dreadnought parity is not the issue, of course, because nobody else has any. Critics of the battleship reactivation program consider this in itself to be a fairly eloquent fact. The battleship is primarily a target today, the argument goes, and even if it could be written off as only a frigate-scale investment, there will be a crew of 1,600 (some 700 less than the WW II complement) to consider. The Navy will have to put so many resources into protecting it that it will actually detract from the overall seapower mission. Furthermore, the critics contend, it is a tempting target, capable of eliciting not only the obvious Soviet countermeasures but also sea- or airborne equivalents of Khaddafi-crazed hit teams-remember the Maine?-in any corner of the globe. Such is the liability of residual dreadnought symbolism.

"I've heard it all," says Cmdr. Jack Dennis from his Pentagon office. Dennis is the officer in charge of explaining born-again battleships to the world. He explains that an onthe-horizon presence—parked ominously just off, say, El Salvador—is on the list of battleship missions, but well down in priority. "In any case," Dennis notes, "that would be a political and not a Navy decision."

The more likely mode of deployment of the lowa-class battleships today would be within what the Navy calls a Surface Action Group (SAG), in which a guided-missile cruiser and various smaller escort components would offer the integrity of antiaircraft and antisubmarine defenses (illustration, page 74). Should there be a serious escalation of threat, either worldwide or in a particular high-seas environment, the SAG could be folded into a Carrier Battle Group (CBG), further affording a 500-mile air umbrella, on the average, over the entire flotilla. In the Navy's view, the amount of sheer offensive punch an updated battlewagon would add in these situations is well worth the additional defensive considerations.

In the case of USS New Jersey, due to be commissioned next January following sea trials, the initial-phase "upgunning" will include:

■ Tomahawk missiles—32 of them in eight sets of armored launching tubes. These are cruise missiles operating on a turbojet engine after

being launched by solid-propellant rocket, with both land-attack and ship-to-ship capabilities. A (tactical) nuclear-tipped version for land attack would have a range of as much as 1,700 miles; the conventional land-attack version ranges to about half that, and the antiship version can be launched at a range of just under 300 miles. In recent tests, according to McDonnell Douglas, a guidance supplier, land-attack Tomahawks have regularly impacted within a few feet of targets after long-range cruise flight.

■ Harpoon antiship missiles—16 in four quad launchers. These attack hostile ships at about 70 miles, using their own active radar after launch on an approximate target bearing. In the terminal flight phase, after cruising near the surface, they fly a "pop-up" maneuver to foil enemy defenses and enhance their effectiveness.



Visitor gets a taste of hard time in one of lowa's cells. Battleship's brig offers semirecumbent accommodations for six.

■ Vulcan/Phalanx close-in weapon system-four mounts. This is the melding of sophisticated radar tracking and electrohydraulic aiming system with the Gatling-type, six-barrel, 20-mm cannon automatically firing a heavy (uranium-densified) projectile at 3,000 rounds per minute. (In fact, this little gun could get off a higher total weight in rounds over extended times than a 16-incher in the main battery can.) It will be the battleship's primary defensive weapon, able to acquire cruise missiles (or aircraft) on its tracking system at about six miles out and begin rapid fire at about three. As formidable as this sounds, it is well known that the Navy would like something better for "point defense" of ships. Nonballistic systems using laser or particle beams are being researched.

■ Helicopter flight deck on the fantail, with storage area for up to three battleship-based 'copters adding a dimension in over-the-horizon surveillance and targeting to both missiles and conventional ordnance.

In view of this high-tech reworking, it is rather remarkable that anyone can get excited about something so primitive as a gun, but any Navy man who has seen it fired in anger apparently remains permanently in awe of a battleship's "main battery" (it won't be called that any longer). Completing the anachronism, those 16-inch guns on the New Jersey will still be aimed by the 1930s-design mechanical fire-control computer in which not only target range and bearing, but also the rotation of the Earth (still the same old Coriolis force), are ingeniously reduced to a matter of gear ratios, handcranked to an instantaneous sum.

Out of mothballs

There are some two dozen unused 16-inch gun barrel liners available from storage depots in the southwestern desert and in the Philippines. Although no facility now exists to manufacture ammunition for these guns, there are about 3,000 armor-piercing and 15,000 highcapacity (less metal, more explosive) rounds in Naval inventory. Although some were 30 years old when loaded for the New Jersey's Vietnam tour, they were found to be generally in good working order: Plop one high-capacity round 20 miles away into dense jungle and, voila!-Instant Helicopter Landing Zone (carefully avoiding the imposing central crater) of some 200 meters' breadth, with a generally defoliated perimeter extending out another 100 to 200 meters. Once the battleships are out there again, it's unlikely that the Navy will be able to resist fooling around with adding a "smart" dimension—final trajectory control-to 16-inch ordnance. Lilies were made to be gilded.

Six of the original 10 five-inch twin gun turrets will remain on the New Jersey. These guns have a 10-mile range for shore bombardment, but can also be used against aircraft. (The first time proximity-fused projectiles were ever fired at sea, aboard the USS South Dakota in October 1942, she shot down 26 ene-

my aircraft in one day.)

Although it is a 600-pound system, and most of today's Navy ships run on 1,200-pound steam turbines, the Iowa-class powerplant is by no means a gross anachronism. Eight boilers feed turbines to provide a total of 212,000 hp to four propeller shafts. Conversion from "black oil" to standard Navy fuel (diesel distillate) will be the bulk of powerplant update work.

The "habitability" area is the one in which the battleships most resemble time capsules (with exception of

BOX OF AIR WITH ANY PAIR.



found plenty of peeled paint; it only means the dehumidification system is doing its job.

Yet appearances, in a larger sense, are what may determine whether the Navy wins Congressional approval to reactivate all four Iowa-class ships. If New Jersey's trials and recommissioning prove at least uneventful, the plan stands a chance. But battleships, historically, have tended to attract the wrong kind of attention. Beautiful at sea, they are brutes in close quarters; Missouri's losing encounter with the bottom of Chesapeake Bay in 1950 sticks in more memories than the landing at Inchon of the same year. Wisconsin wears the bow of her never-completed sister ship, Kentucky, affixed as a major repair after she rammed a destroyer in 1956.

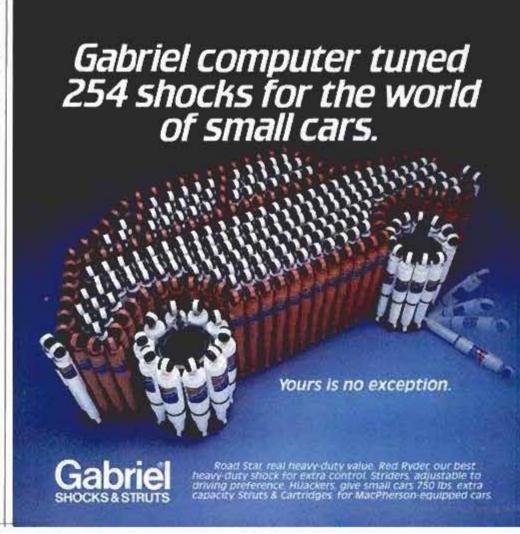
It would be worse yet to be hounded down by inferior forces-as dreadnoughts often were in war-to take some popgun nation's lucky shot in the magazines where 1,005 16-inch shells and their propellant charges (a "full bag" for an Iowaclass battleship) are stored. This would be an incalculable blow to national morale.

Small chance? Perhaps, but in any case, it's one the Navy is ready to

New Jersey). Today's Navy tries not to stack seamen's bunks more than three tiers high-they're five up on the BBs-and, of course, they must have provision for "privacy cur-tains." The galleys will need to be equipped to produce the fast-food items modern sailors prefer; space will be reclaimed from the admiral's quarters to provide for a completely electronic Combat Information Center, soul of a modern fighting ship.

While things are looking up for seamen, and down for admirals, the question of whether a battleship will still need a six-cell brig-bigger than most small-town jails-has not yet been decided. An earlier decision to strip what could be as much as \$50,000 worth of teak from the decks of reactivated battleships-giving rise to a sudden interest in woodcraft among Naval shipyard employees-has been reversed. Teak will come off the fantail, in the helicopter landing zone, but will be used to patch other deck areas. The timehonored practice of "holystoning" teak decks, however, is no part of today's Navy, so the decks will stay bone gray in appearance.

Appearances can be deceiving, however, particularly in regard to mothballed ships. Walking through the lowa recently, you would have

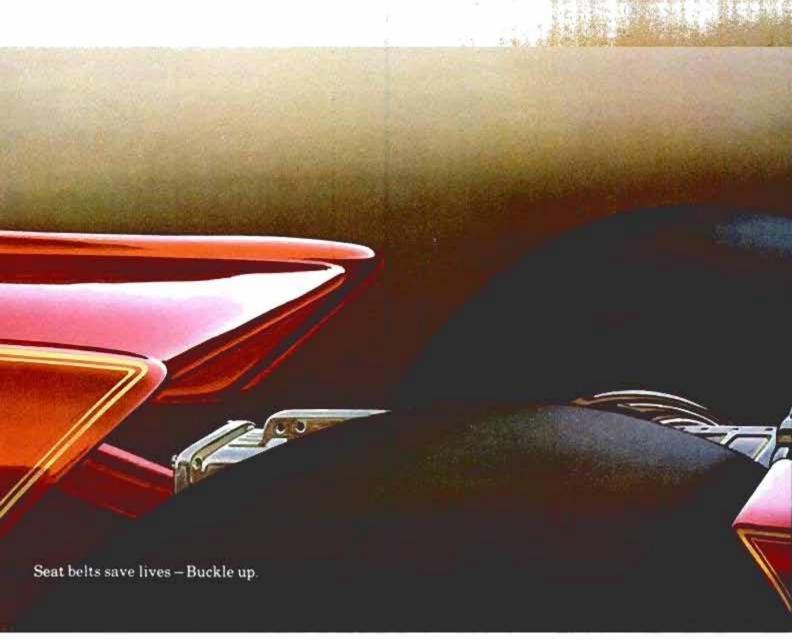


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FIRSTHAND REPORT

Audi Quattro: Germany's 4x4 Cruise Missile

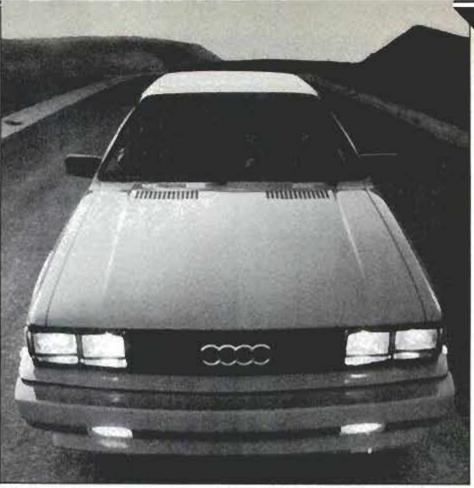
Four-wheel-drive doesn't necessarily mean getting your hubcaps dirty.

by Tony Assenza AUTO EDITOR

The Audi Quattro is the pet project of one Dr. Ing. Ferdinand Piech, who, it happens, looks a good bit like Frank Perdue and is possessed of one of the gentlest souls you're likely to encounter anywhere. Unlike Perdue, who traffics in poultry, Ferdinand Piech, Audi's head of research, deals out tactical weapons for the open road.

Herr Piech's latest exercise in road-going exotica reads like an incongruous concoction—the genetic splicing of a 4×4 and a sports coupe.

The pieces that make it work are as follows: a 2.2-liter, 156-hp, fuel-injected, five-cylinder engine fitted with a turbocharger and an intercooler, connected to a four-wheel drive system which distributes traction evenly to the four corners without benefit of viscous coupling. Add-



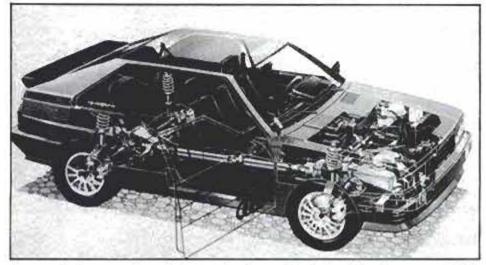
Made for the open road, the Quattro has a top end of 128 mph and it gets there quickly.

ed to this all-wheel drive system is a differential lock feature, which allows you to lock the rear and center differentials. As we discovered trying to climb a mountain road covered with two inches of ice, this locking system works like road glue. The differentials are locked by pulling a knob on the center of the console. When the two lights flanking the knob light up, that's your indication that the differentials are locked.

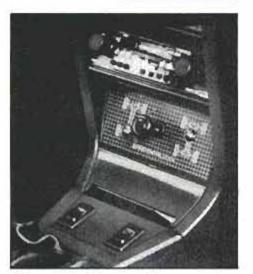
Unlike most two-wheelers converted to four-wheel traction, the Quattro doesn't penalize a low center of gravity by sitting any higher off the pavement. Incredibly it has the same five inches of ground clearance as the Coupe on which it is based.

On dry pavement, in the fully locked mode, the Quattro is transformed into a gravity-enhancement machine. Thanks to the low center of gravity, all-wheel-drive and the 205/60 × 15 Goodyear NCTs, the Quattro feels as if it's defying the laws of physics. Although it feels quite good without the lockup engaged, the car seems to only really come together when it is engaged. Although Audi suggests using the lockup feature only on slick roads, I have a feeling

(Please turn to page 148)



Compact 4×4 system allows the Quattro to sit a mere five inches above the pavement. This makes it strictly an on-pavement vehicle. Suspension is independent at all four corners.



Differential locking knob is mounted on console; lights show system is activated.



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AUDI QUATTRO

(Continued from page 147)

that more than one of the 500 lucky people who will own a Quattro this year will pull the knob whenever the spirit of Fangio moves them.

Amazingly, Audi claims that the full-time 4×4 system doesn't penalize gas mileage. According to its tests, there is only a three-percent fuel penalty at low speeds, and virtually identical mileage to the frontdrive Coupe at high speeds. This is because a driven wheel has less rolling resistance than a nondriven or freewheeling wheel.



Fender flares and logo on door are only indications that this isn't fwd Coupe model.

Freewheeling or not, the Quattro can cruise at elevated speeds for hours on end without flagging. In Death Valley, where we tested the car, we had a chance to mash the throttle and watch what happened. What happened was a calculated speed of around 128 mph-calculated because, as you might know, government-approved speedometers only register to 85 mph. At top speed, the Quattro does tend to get light in the front end.

The Quattro shares with the Renault Fuego (see story on page 90) the distinction of being one of the only two turbo cars on the market with an intercooler. It does what it sounds like-cools the boosted air between the compressor of the turbo and the intake manifold.

Intercooling alleviates the main problem with turbo engines-heat. Excessive heat causes a variety of problems, the worst being a tendency to detonate. Without an intercooler, the only ways of controlling detonation are by feeding the engine an overly rich mixture of fuel to cool the charge, or retarding spark timing (equally wasteful because it reduces thermal efficiency and results in a loss of power). The intercooler lets you run a fairly lean mixture without fear of detonation and doesn't require greatly retarded timing.

The cost for all this marvelous technology is about \$38,000. Audi will only import 500 units this year, so if you want one, you'll have to hurry. See your local Audi dealer and tell him Dr. Piech sent you.

Ott

S18 (3rd Class-US only)

PM's LONG-TERM CAR TESTS

(Continued from page 89)

tremely hard, enjoying its fine performance and lively acceleration with lots of heavy-footed action. Still, we've managed to average 21.7 mpg. Other than the spoiler, nothing else has fallen off or acted up in any way. We'll let you know how things progress.

Toyota Celica Supra

Since about 25 percent of all cars sold in this country are imports, we're going to keep about the same ratio in our long-term test fleet. Our first import is Toyota's newest supercar, the Celica Supra.

Choosing this car was, admittedly, purely subjective. We liked the way it looked and wanted to find out if it had the performance, handling and

durability to match.

We picked up our car at Toyota's East Coast headquarters in West Caldwell, N.J., on March 3, 1982. So far, we've only put 100 or so miles on the car so really, there's nothing to report. We can tell you that, next to the Camaro/Firebird twins, this is one of the best looking vehicles extant. And even going through a slushbox (automatic transmission), the dual-overhead-camshaft, 2.8-liter engine yields sparkling performance and decent mileage.

It's equipped with just about every available option, including airconditioning, power seats, a killer of a stereo system, remote outside mirrors, full instrumentation and digital readouts plus the graphic speed display. The list price of the car is \$15,610, although we've seen Supras discounted in the New York area for \$12,900.

In our next report, we'll have more details on the Supra's performance, handling and fuel mileage, as well as how the car has held up after some hard driving at the hands of the PM staff.

The 5.0-liter HO Mustang

Whereas the Supra is sophisticated and subtle, the Mustang is primitive and a bit brutal. It is precisely because of those qualities that we chose it for long-term testing. You can order it any way you want, from a thrifty 2.3-liter Four to the 161-hp, 5.0-liter, high-output V8.

The only options we chose were those which enhanced performance, rather than creature comforts. Those options were the TRX suspension package which includes Michelin TRX tires, alloy wheels, stabilizer bars and specific shock absorbers. We also added Recaro seats, and the four-speed manual transmission.

Other than that, air conditioning and a leather-wrapped steering wheel are the only comfort options. With all of that, the retail price is \$8.682.

So far, we've put 2,362 very hard miles on the car and the mileage has been a very respectable 14.7 mpg. Not bad, considering all the tire blistering burn-outs and redline runs through the gears at every opportunity. We're not exactly beating on the Mustang, merely using it in the way it was built to be used.

It has consumed precisely 11/2 quarts of oil in 2,362 miles and has

developed only one peculiarity. It will occasionally refuse to idle until it's thoroughly warmed and at other times it will idle at 1,400 rpm for a few minutes. These anomalies disappear as quickly as they come and a visit to the local dealer found all systems normal. Curious.

Those two problems notwithstanding, the car has started promptly in the most frigid winter mornings, run unfailingly strong and given hours of pleasure on spirited back-road missions. The one other problem area is the car's tail-

(Please turn to page 150)





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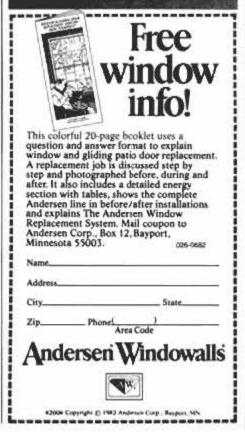
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PM'S LONG-TERM CAR TESTS

(Continued from page 149)

happiness when powering out of a corner. Due to the light rear end and heavy front end, the instant horsepower conspires with the solid rear axle to move you sideways. To overcome this problem, you'll have to steady-throttle the car through a corner and get back on the gas only after you've gotten the car unwound and back on the straight.

Ease of service is something which Ford seems to have on its list of priorities because the engine box is one of the neatest and least cluttered we've seen in a long time, especially

for a V8.

We'll bring you a further report on the Mustang when we get more miles on it.

Pontiac A6000

The GM A-cars are the most significant and, quite probably, the best cars the General has ever produced. Unfortunately, they were intro-duced simultaneously with the Fcars (Camaro/Firebird) and those two vehicles have stolen a lot of publicity thunder from the A-cars.

We've barely had a chance to turn a wheel on the 6000, so there isn't much to say in the way of mileage. durability or behavior in daily commuting. We can tell you that it is optioned up almost to the limit, with power seats and windows, automatic transmission, super deluxe interior, high-dollar stereo, sport suspension and more. It's powered by the TBI Four which, so far, has yielded close to 20 mpg. A lot more on this car to come

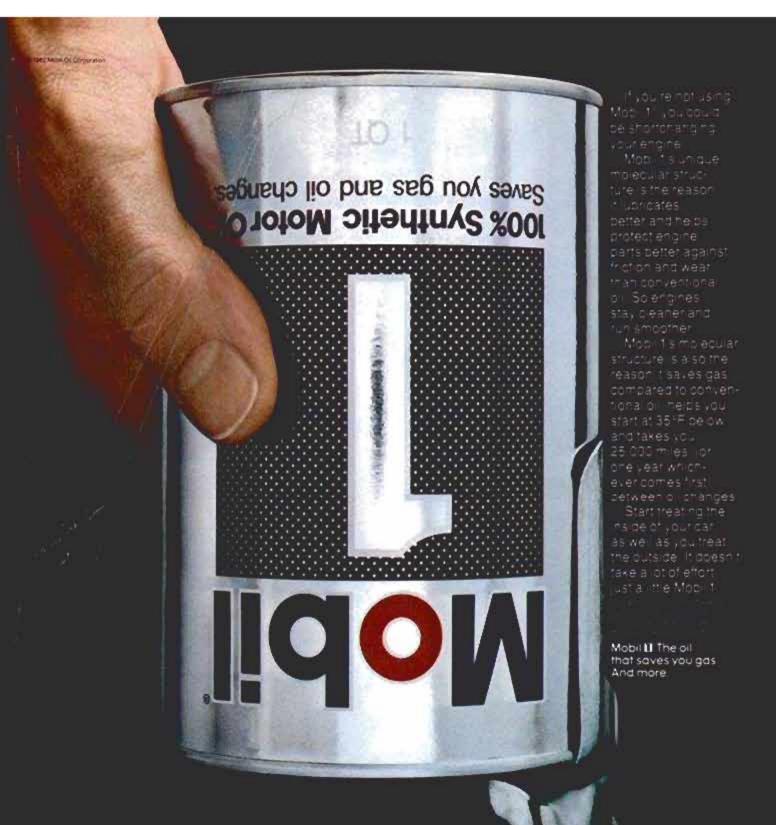
WORKSHOP MINICOURSE

(Continued from page 100)

thinned a bit with kerosene is perfect) on the stone to float metal particles to the surface and to keep the stone's pores from clogging. Remember that a plane iron's cutting edge actually should have two angles on it-the grinding angle and the whetting angle (see drawing).

In practice, when using the oilstone you should only be touching that portion of the blade necessary to create the whetting angle.

Use a perfectly flat stone and keep the edge of the iron flat as you move the iron in either circular or figureeight motion. After you have whetted the 30° angle on the edge, you will notice a slight wire curl on the flat side of the blade. This must be removed by making several passes of the plane iron, flat side down as shown.



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TECHNOLOGY UPDATE

It came from Down Under

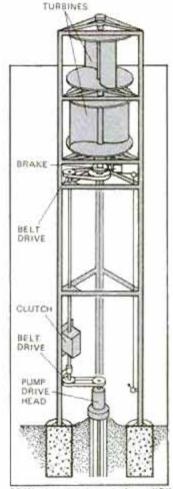
Inventions

Tinkering and inventing are more than a national pastime in Australia; there's even a television show that celebrates inventors and their hardware. At the recent TechEx technology transfer expositions in Lyon, France, and Atlanta, Australian innovations were on hand in abundance. Here are some award winners.

The Marine Thermal Auto-Pilot signals a helmsman when a vessel approaches a current of a desired temperature, such as the critical 72° to 75° F. range required by the tuna industry.

The device is able to interface with an automatic pilot to keep the boat within the thermal current. offering considerable savings in fuel. Temperature probes in stainless-steel cases trail in the water on either side of the boat. The 12-volt circuitry operates on a window principle. The operator sets a central temperature and the system steers toward the warmest probe. Developing company is Michael Australia, Box 82, Desmond, New South Wales (N.S.W.) 2299.

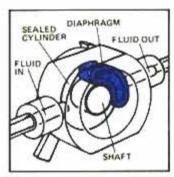
The Mono Wind Turbine offers a step up in pumping power for agricultural applications. It couples "S" rotor turbines (efficient at low wind speeds) to a positive displacement helical rotor pump through belt drives and a special clutch. The latter, a centrifugal type, engages (at about twice its cutout speed) when the turbines have stored energy in low



Mill's extra power is from "S" rotors and special clutch.

winds. The system slows under the drag of the pump, then disengages and the cycle begins again. In moderate (15 to 30 mph) winds, performance is continuous and doesn't need reduction gears to achieve starting torque in low winds. Unisearch Ltd., University of New South Wales, Box 1, Kensington, N.S.W. 2033, is licensing the technology.

The MacLeod Rolling Diaphragm Valve is based on a sealed case containing a short section of silicon rubber tubing which rolls around a central shaft, sealing or opening the valve body. Since there is no sliding friction, wear is reduced; disassembly for maintenance is simple. The positive pressure seal of the diaphragm makes the valve appropriate for a wide range of applications—from domestic water taps to anesthetic gas control, almost anything, except extremely high-pressure or solvent fluids. Design & Technology Ser-



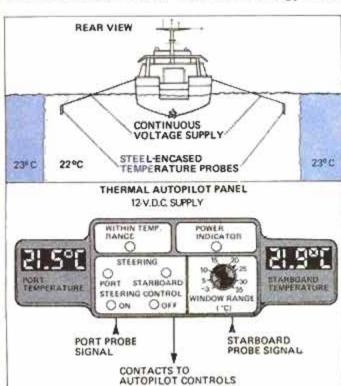
Rolling diaphragm valve: postive seal without any friction.

vices Pty./Ltd., 225 Colchester Rd., Kilsyth, Victoria 3137 is the licensor.

The Push Cush was invented by housewife Mrs. Joan Stuckey. It's a two-section plastic cushion with a restrictive orifice between sections. The user inflates it partially, then pushes air from one side to the other with the feet. Airplane travelers, arthritics and others have hailed it as the best take-along therapy. Push Cush Co. is at 1166 Burns Rd., Turramurra, N.S.W. 2074.



Push Cush has been hailed as best therapy for aching feet.



Thermal steering system allows fishing vessel to find desired currents according to water temperature and stay with them.

TECHNOLOGY UPDATE TECHNOLOGY UPDATE

Aviation

Everyman's crop duster

Crop-dusting airplanes come in odd shapes, but this may be the strangest of all—at least to insects lurking on the ground. Looking a bit like a giant bug itself, it's a modified Goldwing ultralight aircraft fitted with sprayers that dispense pesticides, herbicides and other crop-control chemicals.

Dubbed the Gold Duster, it's said



Prop-driven atomizer varies size of spray droplets to control bug-killing pattern.



New role for an ultralight: Spewing twin streams of insecticide, 30-hp Gold Duster covers 40-foot swath at 60 mph. costs \$6.500 as kit, is exempt from FAA licensing.

to be ideal for aerial bug spraying because it can hedgehop close to the gound and land in a peapatch. But the best news for farmers is that it comes as a low-cost, build-it-your-self kit and, under current FAA regulations, its pilot requires no license (though flight instruction is a requisite for an inexperienced owner).

The Gold Duster can cover a 40foot-wide swath at up to 60 mph, dispensing up to six gallons per minute
from each of two spray heads
mounted on a boom under the fuselage. In each spray head, a tiny, variable-pitch propeller, spinning in the
slipstream like a pinwheel, drives a
rotating atomizer. Adjusting the

blade pitch varies droplet size from 80 to 1,000 microns. Thus, the spray pattern can be tailored to type of crop, wind and weather conditions.

Originally designed as an ultralight sports plane, the Gold Duster employs a sophisticated three-axis control system, spoilers and an antistall canard wing on the nose—features well suited to the slow, tight maneuvering of crop dusting. (For more on the Goldwing, see 14 New Ultralights You Can Build From Kits, page 94, May '82.)

It's sold in kit form for \$6,500 and complete for \$9,000. An info pack is \$6 from Goldwing Ltd., Box 1123, Jackson, Calif. 95642.

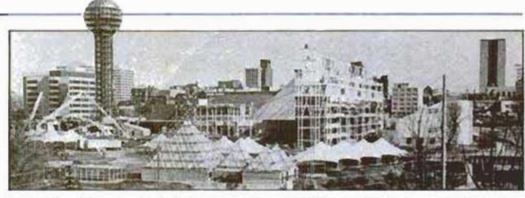
Attractions

A Fair deal

The 1982 World's Fair opened recently for a sixmonth stint on a 72-acre fairground in Knoxville, Tenn. Following is a rundown of some technology on display:

Japan—Of the 21 nations taking part, the Republic of Japan steals the show with its industrial robots. The world's robotics leader shows how the automated manipulators perform. Don't miss the unique dome-screen movie theater with a moving floor to transport viewers through Japan's technical history.

Saudi Arabia—The preeminence of petroleum in the Middle East energy pic-



Gilded glass "Sunsphere" dominates energy-themed 1982 World's Fair, just opened in Knoxville.

ture has not prevented the Saudis from developing alternatives. The exhibit focuses on domestic uses of solar energy, creating an authentic desert environment.

China—The Chinese consistently tie past and present to the future, so the use of solar energy to power an antiquated, 20-foot "dragon boat" on the Ten-

nessee River seems in keeping. Making propane from local marsh gases is another energy-related demonstration.

Australia—The Australian exhibition houses solar water heaters and a whole family of futuristic windmills that irrigate exotic Eucalyptus trees and similar transplanted flora. United States—The most impressive pavilion in size and design is the energy-efficient U.S. hall, with 42,000 square feet of exhibit space. The building's roof is a 5,000-square-foot solar collector. And there's a 65- by 90-foot IMAX movie theater, the world's largest, which seats more than 1,200.

The series of exhibit lev-

TECHNOLOGY UPDATE TECHNOLOGY UPDATE

Design

6 rms, riv vu

We're all familiar with the mobile home as a way to have your house and take it with you, but Gary Dunne of Patchogue, N.Y., has a new wrinkle on the idea. Dunne built his first floating home in 1971 for his own family. Before he knew it, he was in the business.

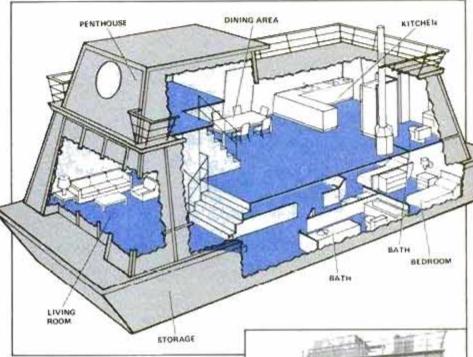
At present, Dunne has completed over 30 homes, from the most popular 1,000-square-foot, two-bedroom, two-story unit to a 1,700 square-foot version with three bedrooms, two baths, a third-story penthouse and a cathederal-ceiling living room.

Dunne's floating house differs from a houseboat because it has no internal power and must be towed to its dock. The homes meet all maritime regulations for floating structures, as well as standard residential building codes, so an owner could lift his home out of the water and set it up on a dry-land foundation.

The house itself is built on a flatbottom plywood and fiberglass barge which has a draft of only 8 inches. The 4-foot-deep barge is 14½ × 40 feet for the 1,000-square-foot version and 20 × 40 for the three-story model.

The hull is built of pressureimpregnated beams skinned with 1/4inch exterior plywood which has been sealed with a polyester resin. Hot-dipped galvanized nails are used throughout. The whole unit is fused into a monolithic structure with three layers of fiberglass and polyester resin.

Dunne says the key to making a floating house work in terms of comfort is extensive insolation and seal-



ing. He uses double-paned tempered glass for the extensive glass walls used throughout the structure. The ceiling is insulated to a full R-22 value and walls and hull are covered with double-foil-faced fiberglass for an R-11 value.

Warm air is ducted through the house by a propane-fired hot-air furnace. Dunne claims this isn't needed except in the evenings and on overcast days: The large amount of southfacing glass serves as a passive heating system. If the south-facing windows supply too much heat, there is nothing to stop an owner from turning the house around during the hot months.

Mooring is by a pile-and-ring system that allows the house to rise and fall with the tide, but prevents almost all rocking and surging motion.



Builder Dunne (above) with penthouse model floating home. Planned luxury version (below) includes an indoor pool.



els show America's energy history in fabulous, eyefilling detail.

Symbolic of the '82 Fair is the 266-foot-high Sunsphere, a spectacular 24carat-gold-coated, energyefficient glass globe, that houses a split-level restaurant and three observation decks.

The Fair is the most energy-efficient way to see how the world plans to cope with its energy needs—at \$9.95 for a one-day admission.

While most of exhibits deal with the future, a brewerysponsored show recreates a pre-Civil War foundry.



Harvey Shaman

TECHNOLOGY UPDATE TECHNOLOGY UPDATE

Electronics

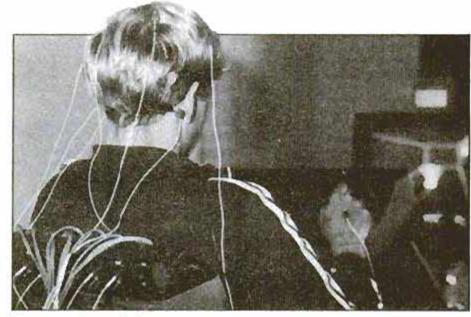
The brain trigger . . .

Move over, Freud. The story from the Psychology Dept. of the University of Vienna these days involves sophisticated electronic measurement apparatus and on-line computer monitoring of human brain functions. With systems capable of measuring a d.c. potential change of as little as I microvolt (.001 volt) within the cerebral cortex of subjects performing perceptive (reading, memorization) tasks, a major phenomenon has been confirmed:

When there is a slight negative shift in d.c. potential in the cortex, a person's learning ability improves immediately by 25 percent. This lets a subject absorb 25 percent more material in a given time, or a given amount of material in three-fourths the time.

So sure are Dr. G. Guttman, department head, and his colleagues of the advantages of learning during cortical negativity, that hundreds of Austrian schoolchildren now spend part of each school day doing relaxant and meditative exercises (see photo) to induce this tiny, critical negative shift in brain electricity.

Guttman's group developed the



Austrian pilot candidates must solve a series of computer-generated spatialrelation problems (right), while working against resistance machines. Some bomb out under the additional physical stress.

exercises through the use of biofeedback techniques: Negative d.c. potential is picked up via electrode, amplified and converted to a tone which the subject learns to control through trial and error.

. . . And 'champions' who shoot blanks

In a related branch of brain-activity monitoring, the Vienna Psychology



group has developed computer software that presents a subject with perceptual problems while he is undergoing physical stress (on a bicycle ergometer, for example).

Some can hack it; others bomb out on arcade-game type maneuvers that they can perform easily without the added stress. It's led to a system that identifies the "training champion"—the guy who shoots 80 percent from the foul line in practice, but can't convert one out of two in a pressure game situation. And the tests are remarkably consistent: Subjects whose performance isn't altered by added physical stress occasionally even improve slightly; the failures never do.

An outgrowth of these stress games is now used to screen Austrian Air Force pilot candidates, and other NATO military services are considering it.

Is there any hope for the "training champion"? Possibly. Stress performance could be related to the same brain-current shift that regulates perceptual ability—biofeed-back training may apply.



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20M282

SUPERCHARGE YOUR CAR STEREO

(Continued from page 80)

balanced, with superb radio reception, but it had definite sonic limitations. Here's how we improved it.

We began by extending the frequency range of the system with additional speakers. Most originalequipment car speakers and medium-priced aftermarket units have acceptable response and power-handling capability in the middle ranges; it's in the deep bass and highest treble that their performance is lacking.

To remedy this, we followed the same approach as designers of large home speaker systems: We used specialized speaker drivers that handle the extremes of the music spectrum. At the bass end, we added an Altec SW-1 subwoofer system (\$239.95), which is a 6 × 9 driver with its own built-in amplifier and associated dash-mount control box.

The SW-1 is designed to reproduce the range from 20 to 200 Hz, using what engineers call a "long-throw" cone design-a cone capable of moving long distances in and out, thereby moving enough air to pump out

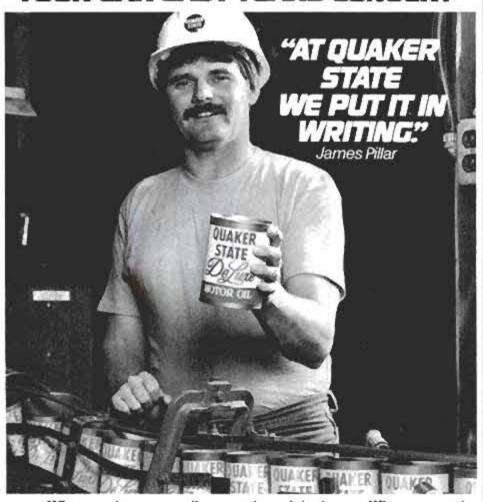
high-level bass with minimal distortion. It can be mounted in any midsized enclosed space: trunk, footwell or even rear side panel, with its control unit up front within easy reach. We dropped one into place behind the seats in the 450SL with about two hours' work. The differences in deepbass solidity and "oomph" were obvious as soon as we cranked it up.

At the other end of the musical spectrum, a pair of add-on treble units were the key to better highend definition and a more convinc-ing stereo "image." The idea here was to put the two treble units up as high as possible in the vehicle interior, so that their output blended with and augmented the treble response of the main speakers.

We mounted a pair of Philips 8320 Tune Up Tweeters (\$79.95 per pair) forward in the doors. These speakers consist of 1-inch dome drivers in a sleek, high-tech case with a built-in thumb-wheel level control. In place, their Eurostyled appearance looked just right, and the source of sound had moved upward and acquired a new sharpness-the music now seemed to come from a broad area somewhere on the hood. (All doormounted speakers, by the way, should be located as far forward as possible to minimize the impact of the slamming doors.)

Another deficiency of the car's original sound system had been a lack of power-the Alpine has only 4 watts per channel built into its chassis. (The manufacturer expected most purchasers to use the cassette deck in conjunction with a separate power amplifier.) The owner of the Mercedes had opted to use it without external amplification. Our interest in setting up the system for wider range sound, though, made a separate power amplifier a necessity.

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STAYIN' ON THE ROAD WITH OLIAKER STATE

Power to burn

Though only a single, two-channel power amplifier was needed, PM fitted two power amps (shown in the photos on page 81) to illustrate the two basic installation options available. As one approach, we mounted an Alpine 3002 50-watt-per-channel amp in the trunk, fastening it to an inner side panel and routing the speaker and power cables back through the rear panel to the passenger compartment.

As an alternative approach which might be used in a hatchback, a station wagon or a subcompact car-we installed a low-profile, 50watt-per-channel ADS Power Plate 100 amplifier (\$320) under the driver's seat, routing its wiring beneath the floor carpeting to the dash. Both of these locations provide the modest amount of air circulation needed to keep the two amplifiers' operating temperatures within bounds.

Particularly with the convertible top down, the 6 watts per channel from the front-end unit had been adequate for little more than freshair background music. The added power from the external amplifiers made it possible to listen at concerthall volumes and added a sense of unstrained, sonic openness.

In the third phase of our upgrading, we expanded the system's control flexibility-its capacity to be adjusted for proper balance on different types of music. This was accomplished by adding an Alpine 3011 equalizer to the system, mounting it within the driver's reach in the left-hand side of the glovebox. An equalizer is a sort of expanded tone control (see Get Customized Sound With An Equalizer, page 110, Apr. '82). It allows the user to boost or cut the system's tonal response more precisely than conventional bass and treble controls do.

By dividing the musical spectrum into many separate bands, each adjustable with a slider control, an equalizer can be used to balance the deep bass with the mid-bass, the lower mid-range and so on, up through the extreme treble.

The Alpine equalizer has five bands per channel, plus illuminated level meters, and an array of secondary controls to be used with rear speakers (which our two-seater project car didn't have).

You can use a car-stereo equalizer two ways: either as a one-time "fix" for your vehicle's acoustical short-comings, or as a superflexible tone control for the persnickety music listener. In our case, we spent an hour or so adjusting the slider positions, while listening to a variety of musical formats. After that we found that the controls needed little additional fiddling.

We had noticed a lower mid-range hollowness and a bit of mid-treble "sizzle." We were able to rectify both conditions with a few adjustments, further smoothing the sound of what was becoming a first-rate sound system.

Fine tuning

A couple of additional details rounded out the 450SL's sound equipment. To play back the ultrawide dynamic range, dbx-encoded cassette tapes the car's owner had made on his dbx-equipped home cassette deck, we added an automotive dbx Model 22 decoder to the array of "black boxes" in the glove compartment. We used it in lieu of the Dolby noise-reduction system built into

the Alpine unit, but only on cassettes encoded in the dbx format.

We also replaced the stock Mercedes antenna—which mounts on the right rear quarter—with an extremely sensitive Hirschmann 6900U electronic antenna. It incorporates signal-boosting circuitry built into its cast housing and has a whip length of only 18 inches.

And to connect all of the elements, we used two types of premium low-resistance Monster Cables (that's the company's name, honest)—a heavy-duty, double-jacketed speaker cable that goes by the name Hot-

wires, and a thick shielded wire, called Interlink, for low-level signals. Both are more weather-resistant and freer of stray electrical gremlins like excess capacitance and inductance than conventional wiring. We crimped the speaker connections to spade lugs at each end and covered all wiring splices with heat-shrink tubing.

The final result lived up to our expectations. The upgraded system's sound had an effortless, spacious quality, along with all of the volume and flexibility that an audiophile demands.

PUT YOUR XACTO BLADE TO THE TEST IF IT CAN'T CUT IT ...CHANGE IT

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Replace only the element, not the whole filter

that you can check the condition of the fuel and the filter.

The finish is hard chrome that reflects heat and resists rust.

ProFuel is a product of CR Industries, Elgin, IL 60120

Easily installed, it's available in auto stores in your area.

PM OWNERS REPORT: MUSTANG

(Continued from page 83)

sion dropping down into low range."

And a North Carolina analyst wasn't alone when she observed, "If I had it to do over, I'd upgrade my current four-cylinder Mustang to a Six. I miss the performance of a more powerful engine."

In the area of quality and workmanship, nearly half our respondents said "good," but fewer than one-quarter gave Ford workmanship an unqualified "excellent" rating. That's the reverse of the marks given the 1981 Ford Escort, EXP and the '82 Continental.

A Texas aircraft mechanic stated, "Body metal is so thin that if you're not careful, you can dent the car just by leaning on it." A Kentucky programmer noted, "The bottom edges of the doors had no paint on them at all!"

And a New Hampshire secretary: "My Mustang has many body leaks. I'm disappointed in the workmanship of a car that is so costly. I've brought it back to the dealer 10 times in 1,600 miles and the transmission still isn't shifting right. I feel Ford should take this car back and start over!"

However, in fairness, we did hear an impressive number of comments from owners who felt good about quality and workmanship. For example, an Ohio hostess: "It's certainly a solidly built car. I rear-ended another vehicle at a red light and the front bumper and hood of my Mustang came out without a scratch. I hit the other car pretty hard, too."

An Illinois airline pilot: "I think Ford quality has improved greatly in recent times." And a Texas auto parts counterman: "My Mustang is so airtight that, even with the T-top, I have to slam the door to shut it against the air pressure inside."

When we asked owners for suggested changes, several said they'd appreciate higher fuel mileage, bet-



Recaro seat option gives support to enjoy handling of optional TRX suspension.

ter workmanship and a lower price tag. Other recommendations included the following:

From a Michigan insurance manager, "I wish they'd change the horn. Pushing inward on the turn lever is a lousy way to honk, even after you get used to it."

A Minnesota machine operator: "I suggest that Ford expand the size of the glove compartment. Nothing fits in it. The trunk doesn't hold much either—too shallow."

"More legroom in the rear seat," suggested a Wisconsin student. And from an Ohio accountant: "Give us some work space under the hood. Adding oil to the six-cylinder engine amounts to a major operation."

General comfort received high praise for the front seats and, considering the nature of the car, fairly high marks for the rear seat, too. A Michigan salesman noted that, "I'm 6-2, and even when I'm driving with the seat shoved all the way back, passengers tell me there's plenty of room in the rear." But a North Carolina systems analyst countered with, "When my rear passengers get their knees out of their mouths, I'll ask them how they like it back there."

A Tennessee student: "Good driv-

Wild Horses To Go

Try to imagine the automotive equivalent of Genghis Khan pillaging a defenseless village and you'll have some idea of what the 5.0-liter HO Mustang is all about. It's loud and arrogant and it will suck the hubcaps off anything that dares to challenge it.

Granted, it doesn't have the sensuous curves of the new Camaros, the technical sophistication of a Porsche or the exclusivity of an exotic. But the fact that you can buy the 161-hp, 5.0-liter for \$8,682 more than makes up for any def-

icit. At that price, it has to be one of the best go-fast bargains on the market. For that low entry fee, you'll have a vehicle that scatters lesser cars before it like a stiff wind through a pile of leaves.

There is room for improvement, however. The wood-grained dash looks out of place in such a muscular car, the steering effort is too light and the clutch pedal effort is too high. But the fact that you can wipe the smile off the faces of Porsche owners makes those compromises seem minor indeed.—T.A. ing comfort, smooth ride for such a short wheelbase." A New York factory worker: "I enjoy my AM/FM cassette deck. The sound is beautiful. Interior feels nice and comfortable, and I enjoy my Mustang's ride and the way it handles."

Only about one-third of our respondents had experienced mechanical problems, and among those, dealer service came in for general praise. The most common malfunctions involved the Mustang's electrical and fuel-intake systems.

In all, then, the Mustang consensus has to be one of owner pleasure and satisfaction. And while only about 75 percent say they'll buy another Mustang next time, most hasten to add that, with growing families, a pony car might not be practical for their next purchase.

The person who summed it all up, however, was a teacher from Utah, who wrote, "I love my Mustang! The luxury and comfort, the smooth ride, handling on curves and slippery roads, the appearance, detail work, service—all these add up to great satisfaction. Besides, the car is fun to drive. I get a lot of compliments and I come from a long line of Ford owners. I'm totally satisfied."

SUMMARY OF 1982 FORD MUSTANG OWNERS REPORTS*

Total miles driven	802,115
Average miles per gallon:	
2.3-liter Four In town	21.0
Long trips	27.8
In town	17.7
Long trips	23.2
In town	16.0
Long trips	22.2
Engine choices:	
2.3-liter Four	54.7%
3.3-liter Six	
5.0-liter V8	0.5
Transmission choices:	
Automatic	67.6%
Manual four-speed	32.4
Why did you choose the A	dustang?
Styling	67.7%
Economy	22.7
Past experience.	126
Price.	10.6

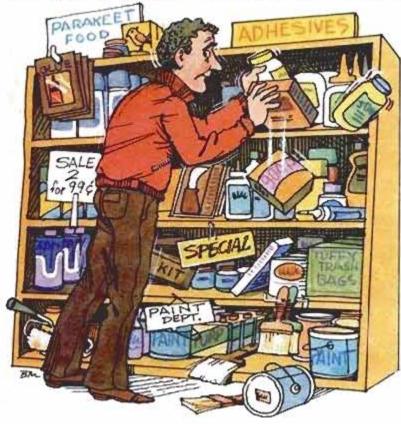
Percentages might not equal 100% due to rou

Specific Illus: Styling. Handling Economy. Camfort. Ride	58.0 45.0 26.0 23.5 14.5
Specific dislikes: No complaints	28.3%
expectation Noise and rattles Price too high Quality and materials	15.2 8.2 7.1 7.1
What changes would you like? No changes. Better gas mileage. Better quality and materials. Lower price. Larger glovebox. Deeper trunk.	21.3% 9.3 8.5 5.9 4.8 4.8
Workmanship opinion: Excellent Good Average Poor	21.6% 48.6 23.6 6.2

Comfort opinion (front seets): Excellent Good Average Poor	37.2
Comfort opinion (rear seats): Excellent Good Average Poor	14.1% 41.8 29.3 14.7
Had any mechanical trouble? No	66.7% 33.3
What type of trouble? Electrical Fuel intake system Transmission Oil leaks Engine runs on (diesels)	19.4% 16.4 14.9 14.9 7.5
Dealer repairs satisfactory? Yes	53.2% 46.8
Dealer service opinion: Excellent Good	34.5% 36.9

Poor	11.3
Number of vehicles owned: Mustang only, Two cars. Three cars. Four or more cars.	44.4% 34.8 10.1 10.7
Makes of other cars owned: Ford Chevrolet Buick Oldsmobile Mercury	64.3% 29.6 11.3 9.6 7.8
Age distribution of owners: 15-29 years. 30-49 years. 50-plus	44.1% 32.2 23.6
Would you buy another Muster Yes	74.7%
Would you buy another Ford po Yes	82.3%

FINDING THE RIGHT ADHESIVE SHOULDN'T MAKE YOU COME UNGLUED.



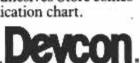
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Everything from our
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cement. You've got it all

right at your fingertips. And to make knowing what you need easy, the Adhesives Store comes with a simple-to-read application chart.

The Adhesives Store. It's at your nearby hardware or home center store.



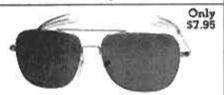
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FREE—limited time only—deluxe velour lined case with each pair of glasses ordered (a \$3.00 value). Credit cards accepted. Dealer inquiries invited.

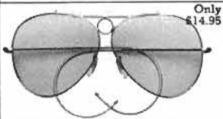
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QUANTITY	MODEL #	GOLD	SILVER	PRIC

COMPUTERS BRING CARS TO LIFE

(Continued from page 79)

ufacturing a steel body from a video framework.

"We're always investigating new areas with the computer," Don Parker told us as we were being shown around GM's Fisher Body Div. "With it, we can find inherently weak areas. Or we can see areas that may not do as much work as other areas—and use mass reduction there."

Put to the test

We watched as a doorpost was put through something called finite regression analysis. The video display zoomed in until only a portion of the doorpost, less than a few inches of it, filled the screen. Then, the computer displayed a network of triangles all along the magnified part.

The woman working the console took control of that doorpost's destiny as she entered several variables via keyboard and light pen to simulate lateral stress on that doorpost. Within each triangular area, she questioned the computer for a detailed printout of the stresses and how they affected the stability of the "metal."

We watched spellbound. Had Detroit forgotten how to make a doorpost? What was all the fuss about?

The idea was mass reduction. Less mass means better fuel economy. By using the computer, designers can find out just how much metal to shave away from the finished version so that it would still support the roof, yet have the least possible mass. Almost anyone could figure out that a thick doorpost will support a roof. But how slender can that post become and still hold a roof above your head? Finite regression analysis can tell you.

The computer helped to shave away a little here and a little there a bit off the top, a slice off the side.

Finally, a computer is used to design the dies from which the actual parts of the car's body will be cast. From clay, through video, to the blazing furnaces of a steel mill, the typical design project takes four years.

There are even more applications of the computer just over the automotive horizon.

At Ford, we watched a video driving simulator. Seated at the console, the operator could "drive" a car through a number of different road conditions. With just a touch of the keyboard, he could drive an entirely different car, or adjust such things as spring rates, shock-absorber rebound and roll steel. When the drive

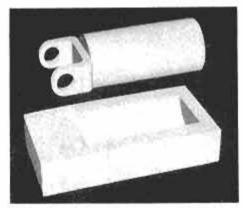
felt good, then a prototype car could be built for road testing.

But while most of today's computer applications involve structural metal design, engineers are also getting ready to design the functioning parts of automotive machinery on computer. Instead of ghostly, wireframe images of cars on screen, there are already computer programs that can display exact three-dimensional representations of everything from hubcaps to camshafts.

Dr. J.W. Boyse, of GM's research labs, explained how the new GM-Solid computer program works.

A collection of "primitive" shapes is stored in the computer's memory banks. These simple, 3-D shapes may be combined into any number of very complex shapes.

Suppose, for example, you want to "drill" a few holes into a flat plate of "steel" on the screen. What you do is to "subtract" cylinders whose diam-



Once you've defined a three-dimensional shape, it's easy to design a shape that it "fits into." The GM-Solid program (above) designs matching machined surfaces.

eters match those of the holes from the plate. Presto. The holes are drilled.

We saw a camshaft model that was as accurate and detailed as a photo of an actual part. But the camshaft had yet to be manufactured and existed only in the computer. Ford uses a similar system to analyze stress.

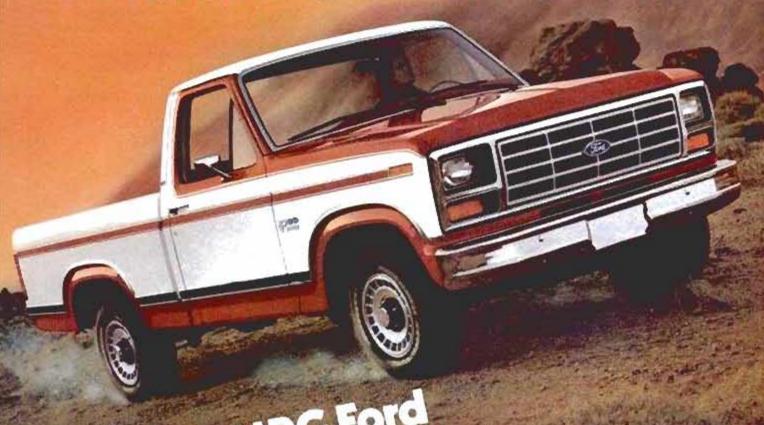
An ever-increasing amount of design work will be accomplished by these computers. Even more exact tests and procedures will be developed. But no matter how important the computer becomes, human ingenuity and appreciation of beauty and utility will be the driving force.

Yesterday's car designers sat in dim classrooms, doodling futuristic autos in their notebooks as the professors droned on. Tomorrow's designers may be doodling, too—and dreaming of a keyboard to call their own.

FREE case with each pair.

City

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New high-MPG Ford pickup rated 22 MPG.

Introducing Ford's new fuel-saving FS pickup: Introducing Fords new fuel-saving FS pickup
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It's a fough, full-size pickup of the comfort for
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It's a fough, full-size pickup with comfort for full-size pi delivers 20 EPA estimated MPG plus 2,500 lbs. of payload. And there's more for '82

Adjustable Twin-1-Beam. Ford's tough suspension is now designed with lubed-for-life ball joints and adjustable camber. New V-6 engine. Modern 3.8L V-6 is a high-technology engine with excellent horse-technology engine with excellent power per liter. Standard in F-100 models. NEW FORD 31 ST PARTY 49LSIX









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Components sense positive pressure (boost) as well as depression (vacuum), making Vara-Jection applicable to turbochargers; mechanical units are not.

Reliable, solid state electronics allows essential functions impossible to achieve with any mechanical water injection system. As vehicle and application vary, so do the amount and timing of water required to control detonation. Vara-Jection assures broad adjustment capability through a combination of fine tuning and nozzle selection. Synchronize water delivery to match your specific engine's needs through the accuracy of

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A complete home workshop in one easy to handle unit equips you to tackle almost any job. Converts in seconds to a powerful 10° table saw, 12° disc sander, 16½° drill press, 34° tathe and horizontal drilling machine—powered by a heavy duty 18 amp ball bearing motor. A variable speed range from 900 to 5400 RPM's allows the precise speed selection necessary for any operation.

Make your own furniture, remodel a room—build any project large or small. You can do it easily and professionally and save money too! Even beginners find themselves quickly achieving beautiful results on every project.

MasterShop is affordable—More value, quality and accuracy for substantially less money than other home workshop systems.

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NO-FUEL BOATING

(Continued from page 93)

folds away, and there are bunks for seven. At about \$1,000 a foot, it's no more than big yachts of years ago.

Meanwhile, Supercats of 15, 17 and 20 feet, have been cleaning up at catamaran regattas. They are built by Boston Whaler Multihulls, at Riviera Beach, Fla., with computerinfluenced elliptical hulls, Prices are \$4,490, \$5,670 and \$9,950 f.o.b.

New \$995 Freesail board-sailers have been announced by O'Brien;



Assorted color combinations for hulls and sails are new for popular Homar Phantoms.



Twin inflated tubes are said to aid the stability of the British Twinsurfer sailboard.



The New MonArk M-1 14-footer has lockers for oars and outboard if the wind fails.

and Kona, in Oak Hill, Fla., has a \$1,995 catamaran sailboard. Twinsurfer is a British inflatable surfboard. Homar Boats, Edison, N.J., has three new Phantom models for under \$1,400. MonArk is introducing a \$2,915 row-power-sailboat.

THE HOT BLAST FROM FRANCE

(Continued from page 90)

many early turbo cars, isn't evident in the Fuego. Boost comes on smoothly at 2,500 rpm and stays steady all the way up to red line. Mercifully, the engine isn't saddled with a rev limiter or any of that nonsense, but has a genuine waste gate to blow off excess boost. The turbo itself is a Garrett T3 unit.

Both engines provide lively performance, though the turbocharged version is, understandably, a lot more fun to drive and yields respectable mileage, even at very high speeds.

There was indication from Renault that a two-liter turbocharged version of that engine might eventually be introduced in the United States. If that does come to pass, those extra 400 cc might very well propel the Fuego into a supercar. The Turbo will be available only with the five-speed manual transmission. The shiftless will rightfully be limited to the normally aspirated version.

Traditionally good ride

The French have always been able to dial in a good ride and the Fuego doesn't break this tradition. The ride quality is firm, yet supple, and fords road irregularities without drama.

However, these were European roads, mind you. The car has yet to be tested in the ultimate trial by crater—New York City streets.

The Fuego styling is something no one will be lukewarm about. Comments from knowledgeable observers ranged from "stunning" to "the hunchback of Malmaison." I liked it a lot in European trim, and, with some reservation, like it in its American version, as well. At any rate, it's a refreshing change from the Japanese folded-paper school of design and the quickly vanishing American chiseled-shoebox approach.

As a strictly technical exercise in packaging, the Fuego is an unqualified success. Even though its wheelbase is a mere 2 inches longer than the R5 (96 vs. 94), Renault has managed to carve out a huge amount of interior space.

The one reservation I have in the American version is the chromed headlight "buckets." By law, manufacturers are not allowed to use flush headlight covers as they do in Europe, a dumb law if ever there was one. But to go with chrome for the bezels is making a bad situation worse. My humble suggestion would be to make them black.

The question that needs to be

asked now is "Can the Fuego find fame and fortune in the United States?"

Judging by how it ran in the highspeed European environment, it's somewhat overqualified for the generally less demanding American driver.

But car enthusiasts should love it. And the price is certainly attractive: \$8,400 for the unblown version and \$10,700 for the Turbo version. With those good things going for it, the Fuego, which is now in AMC showrooms, certainly seems to be destined for success.

SPECIFICATIONS—RENAULT FUEGO

Engine Sta	indard Turbo
Displacement	1.6 liters
Bore	79 mm
Stroke	84 mm
Compression ratio	8.6:1 8.0:1
Maximum horsepower @ 5,500 rpm	B1.53/107
Maximum torque (ftlb. @ 2,500 rpm)	86.29/120
Fuel/air supply Bosch L-Jetronic	fuel injection
Maximum boost	NA 95 p.s.
Dimensions	
Wheelbase	96.1 in
Overall length	176.8 in.
Height	50.5 in.
Tread-front/rear	56.4/53.0
Overall width	66.6 in.
Fuel capacity	14.8 gal
Luggage capacity (back seat up)	13.76 cu. ft.
Tire size—base model	185/70×13
Curb weight—base model	2,379 lbs.

WATER FROM YOUR OWN BACKYARD WELL!

Opelika, Alabama—What can you buy for a nickel and still get some change back? 500 gallons of water, that's what! As a matter of fact, the electricity to pump 500 gallons from your own backyard well costs only 3 or 4 cents!

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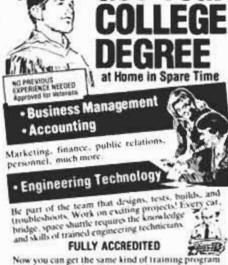
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CAPTURE THE STARS ON FILM

(Continued from page 84)

second shutter speed at F/8 with a 400 ASA-speed film. A half-moon requires an exposure four times longer—1/12% second. Though the moon is in motion, relative to the Earth, exposure times are short enough to produce a sharp image.

But if you want to get photos that take you right into the moon's craters, mountains and valleys, you will need much higher power—and that calls for a telescope with a motor drive (see Backyard Voyage To The Stars on page 86 for more on what these drives do).

The sun

Extreme caution must be exercised in any solar photography. Be sure to use special filters to prevent blindness or camera damage. A beginner should seek expert help.

A solar eclipse is often regarded as one of nature's grandest spectacles. The next major eclipse will occur in Indonesia on June 11, 1983. (If you're interested in seeing or photographing it—with expert guidance—you can write to Sciencefaction Expeditions, Dept. PM, 3 East 54th St., New York, N.Y. 10022.)

The planets

Planets are such tiny objects, as they appear in the sky, that a telescope is an absolute necessity for photographing them with any sort of detail. Venus, Mars, Jupiter and Saturn are within the grasp of amateurs with conventional equipment—the others are too far away, too small or both.

The technique (which can also be used for high-powered lunar photography) is simple, but demanding it's called "eyepiece projection."

The lens is removed from the camera. The camera is held in a lighttight sleeve above the telescope's eyepiece. In effect, the telescope and eyepiece become the camera's lens.

Exposure times range from a fraction of a second to a few seconds. Start with ½ second and photograph in steps to 10 seconds. But expect a high failure rate. Be happy with even one perfect photo out of 36. Vibrations in the mount or motor drive, slight misalignments, atmospheric disturbances—all these variables will be challenging to overcome.

Surprisingly, some of the best photos of this type have been taken in metropolitan areas under smoggy conditions. City lights don't affect short exposures and, because of inversion layers, smog may mean a steady sky.

Comets

It is only rarely that a comet appears that can be seen in the sky without a telescope (Halley's Comet in late 1985, for example). Then a camera and tripod—with the star platform described here—will be enough to capture the image with only a 45-second exposure.

Faint comets, however, require longer exposures. The camera may be mounted piggyback on a telescope, as detailed below.

Galaxies, nebulae, globular clusters and other heavenly phenomena require patience, skill—and a certain amount of equipment.

With a telescope, high power and detail are both available. A 6-inch telescope, for example, has a focal length of 48 inches or 1,200 mm, if it's one of the popular F/8 variety. This focal length makes such a telescope a superpowerful "long lens."

There are two ways of using such a telescope with a camera. The first is called piggybacking. Most telescopes have an optional mount that lets them serve as a driving mechanism for a camera which is mounted on their tube. The mass of the telescope will support a camera's heavy telephoto lens.

But most camera lenses can't use the actual telescope as a lens. Instead, they use a method, called "prime focus," in which the camera is attached to the telescope through the eyepiece tube. (In some telescopes, it may be necessary to move the mirror; instructions are usually included with the telescope.)

Either method requires that the telescope be guided. This is because deep-sky photos can take as long as two hours to expose. Even the best motor drives have some movement error which can smear the image. An eyepiece with an illuminated cross hair is used along with electronic or manual slow-motion guidance to stop the motor drive from wandering.

Long exposures can make film behave strangely. Color balance goes out of whack and the emulsion stops recording the image. To minimize this problem, film is often treated in one of two ways. One method uses a "cold camera" which keeps film at low temperatures but this is quite expensive.

A recent discovery is called hypersensitization. You can get at-home kits to treat film or buy already sensitized film.

Final hints

Process your film normally, and do not "push" the development. And be certain that the first and last photos

(Please turn to page 170)

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CAPTURE THE STARS ON FILM (Continued from page 169)

on a roll are of "normal" subjects (people or places). Otherwise, a lab technician may have difficulty in figuring where your frames begin and end. What a disaster it would be to have a constellation shot split in half and returned mounted as two slides!

Bracket your shots extensively for short exposure work. And make certain you try constellation, comet and

deep-sky photography only on moonless nights and away from city lights.

You might also buy a copy of Kodak's Astrophotography Basics. It's 45 cents from your dealer or direct from Eastman Kodak Co., Dept. 454, Rochester, N.Y. 14650.

Astrophotography is one of the most rewarding facets of a truly heavenly hobby.

BUILD A STAR PLATFORM

ere's a simple-to-build Star Platform that will enable your camera to follow the apparent motion of the heavens. A special motor which turns at one revolution per day lets you use long exposures (45 seconds on fast color film for constellations, around three minutes for deep-sky objects) without images streaking.

Construction

Drill two 1/8-in, holes in the tee plate, as shown on template. Eend the plate with three 90" bends, as shown. Solder a 1/4-20 nut over



the hole (inside the bends) using one of the 1/2-20 brass bolts to hold it in position.

To mount the motor, hacksaw the head off a 1/4-20 bolt which will leave approximately a %-in. shank. Holding the shank in a vise, drill a 1/16-in, pilot hole through shank's length and then a 1/4-in. hole. Sandpaper the motor's shaft to clean it and place this threaded shank over it before soldering it in place at the top of the motor shank. Strip a small amount of insulation from the motor's two wires and solder a lamp-cord extension to them (wrap with insulating tape). Knot a strain relief following the splice. Place the motor "inside" the three 90° bends and fasten with the two 5/2-in. machine screws and nuts. Push the cord through the plate hole; connect a plug. Loosely screw the second 1/4-20 nut down the motor shaft (over the threaded shank).

Tighten the nut against the mount.

With a sturdy tripod, focus your camera on the North Star and lock tripod's pan-head adjustment. Remove the camera and attach the Star Platform to tripod (the motor's axis will now point to the North Star) by threading tripod's screw into soldered nut. Gently attach your camera on a ball and socket mount (see parts list) to the platform-do not force motor gears counterclockwise (as seen from shaft side) or they'll strip. Tighten ball and socket setscrew if so equipped.

Plug in platform motor once you've positioned camera to photograph any object in the night sky. Don't expect to see the motor move, since it is very slow .- R.L.

PARTS LIST-STAR PLATFORM

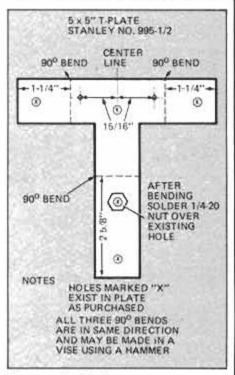
1/4-20 brass bolt, at least 1/2 in, long

1/4-20 brass nut (2)

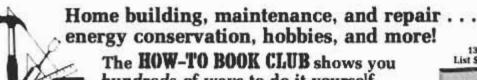
6/32-in, machine screws and nuts, 3/4 in, long (2) 5×5-in, flat tee plate (Stanley No. 995-3/2 or equivalent) Ball and socket camera mount

Two-pronged electrical plug

Synchronous geared motor (should turn one revolution per day and have Ve-in. dia. x Ne-in. length shaft). This motor is \$12 postpaid from Critarion Scientific Instruments, 620 Oakwood Ave., Dept. PM, West Harfford, Conn. 06110.



The text details how to use a template (above) to drill and bend a flat tee plate.



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BACK-YARD VOYAGE TO THE STARS (Continued from page 88,

bit wobbly and the scope wasn't as easy to use as the C-90. The Dynascope RV6, however, made by the same company, was a great buy among Newtonians at \$299.

The other four scopes are made by Edmund Scientific, an amateur science mail-order house that provides a great range of products. The 6-inch Edmund gave us excellent views of the planetary surfaces. Standing on the steadiest of mountings, it was one of the easiest telescopes to assemble. You might wince at the \$479 price tag, but it wasn't out of line for the optics.

Big mirrors, big light

The other Edmunds were less spectacular viewing instruments, largely because their mirrors are less than 5 inches across. The larger the mirror, the better the scope. A 4-inch scope collects 16 times the light of a 1-inch model, meaning distant objects will be 16 times bright-

Large mirrors also have better resolving power-they are better at splitting binary (double) or triple stars, and they provide much more detail of planetary surfaces.

While the three other Edmund scopes provide less mirror than the leader, they have many features to recommend them to the beginner. All three are lightweight and need little or no assembly.

The 2001, Edmund's most popular device, is a perfect field telescope for the novice. Shaped like a basketball, it comes with a carrying strap and weighs 10 pounds complete. Its 41/4inch mirror is ample for starters. The 2001, similar to a Newtonian in design, is a "rich-field" telescope. This means it can show a wider field than other scopes. Of course, this comes at some sacrifice of detail. The 2001, for instance, gave us a view of Jupiter comparable to a good set of binoculars. Yet it gave us brilliant views of wide-area wonders, such as the "seven" stars of the Pleiades constellation, which resolve into scores of luminous star clusters.

The Edmund F10, which also has a 41/2-inch mirror, has features that the less expensive 2001 doesn't, such as a large, sturdy tripod and a longfocal length for sharp magnifica-

Among the smaller scopes, the Edmund F6, with a 3-inch mirror, is

a bargain at \$159. Its short focallength ratio of f/6 made distant stars quite bright. The focal-length ratio expresses the mirror size divided by the distance to the eyepiece. With this f/6 scope, then, the distance from the mirror to the focus point is two inches. Short focal lengths provide brighter images and wider fields of view.

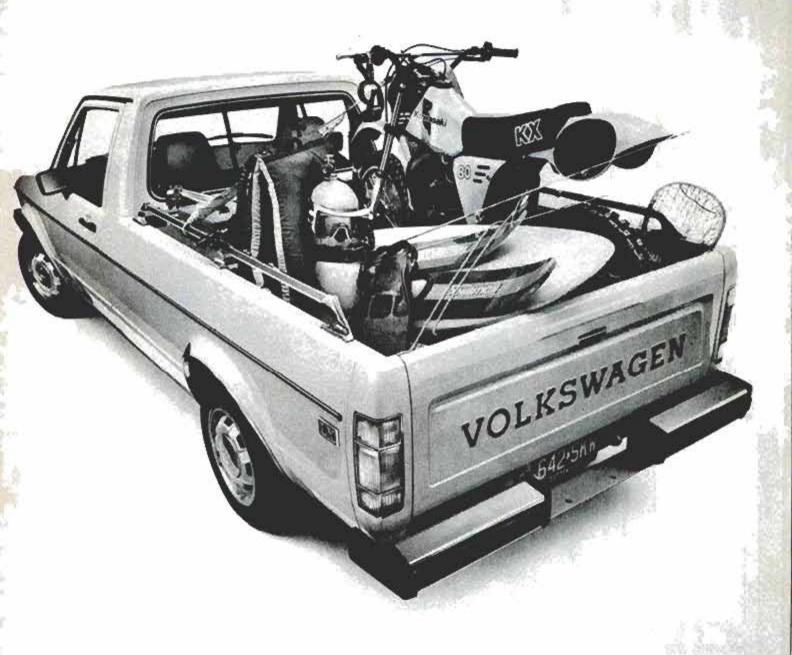
Magnification's no big thing

Many a beginner has been taken in by claims of very high magnification. But in astronomy, magnification is relatively unimportant. In fact, there is such a thing as too much magnification. The lens placed in the evepiece magnifies whatever the mirror is reflecting. To step up the size of the image, however, the lens spreads out the collected light over a larger area. Thus, the object, while growing larger in your field of view, also grows dimmer.

With stars, nebulae and tiny planetary moons, magnification beyond a certain point will blot out the image entirely. For Newtonian and catadioptric scopes, you should allow no more magnification than 50x for each inch of mirror. Thus, a 4inch telescope should have no more

(Please turn to page 174)





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BACK-YARD VOYAGE TO THE STARS

(Continued from page 172)

than 200x magnification. The exception is the rich-field scope, which shouldn't have magnification of more than 10x per inch, unless you're willing to sacrifice that wide field of view for a clear picture of one object.

All the telescopes we tested came with at least one excellent eyepiece. Edmund packs only one eyepiece with each scope. Company president Robert M. Edmund told us: "Generally, when companies pack more than one eyepiece, you're paying for frills you may not want. And those extra pieces are figured into your instrument's price." Although all the extra lenses packed with our test scopes were useful, Edmund is correct about the extras adding unneccessary cost.

All the tested scopes, except the Edmund 2001 and F6, came with clock drives. These motors rotate the telescope automatically and imperceptibly to let you follow a specific heavenly body as the Earth rotates away from it.

Another feature of crucial importance in the better telescopes is setting circles. To find a heavenly body, you refer to a star atlas, a map that uses coordinates of right ascension (similar to longitude), and declination (similar to latitude). Good setting circles should be clearly marked with right ascension and declination coordinates. The Celestron, Dynamax. Dynascope and Edmund pedestal mounts had excellent setting circles

Free looks first

Before you go hunting for a telescope, find the nearest amateur astronomers' society. There's at least one in every state, and if you can't find a listing in the phone book, call the astronomy department of any nearby university. All astronomical societies have monthly stargazing parties to which the uninitiated are invited.

This summer promises a lot of interesting viewing. There are after-midnight meteor showers in June and late July into early August that you'll be able to see with the naked eye. But on July 6, you'll want at least a good set of field glasses, or, even better, a good telescope to see a total eclipse of the moon, which will be visible throughout the continental United States. When the Earth comes between the sun and the moon, the mountains and valleys of the lunar surface resolve in your lens with a clarity you'll never for-

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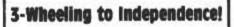
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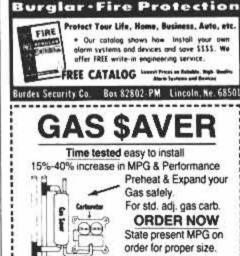
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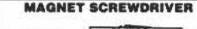


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Caring for your lawn and garden need not be difficult according to Wheel Horse. All it takes are the right tools for the job. 20-page magazine illustrates their full-range of lawn and garden tractors and the jobs they do. Free

567 BACKYARD BEAUTIES

A series of do-it-yourself backyard projects are described and illustrated in a new brochure from Koppers Company. Contains drawings and construction details for a picnic table, lounge chair, garden gazebo, storage shed, sun trellis and more. \$1.00

568 FOR A CLEAN SWEEP

Polaris Vac-Sweep, America's best-selling automatic pool cleaner, offers a copy of Nancy P. Bruning's "Swimming for Fitness," a copy of the independent laboratory test report on Polaris, a general information brochure and \$50 trade-up information. Free

569 THE GRASS IS ALWAYS GREENER

Lawn Care, edited by Scott, the lawn and garden people, contains seasonal checklists, how-to articles and the latest information on lawn and garden care. Now in its 53rd year, a 2-year subscription to this colorful publication is available . . . Free

570 BUG OFF!

New Muskol brochure provides complete information about the original "full strength - DEET" insect repellent. Gives you up to 10 hours effective protection against mosquitoes, black flies, chiggers, ticks and gnats. Free

571 THE TELLING OF THE TILLING

Complete information package describes the uses and features of the Masport Home Gardener tiller. Combines lightweight design, versatility and maneuverability to handle all your gardening chores. \$1.00

572 BE A STAR BUILDER

Countryside General Store offers an information package about its STAR *PLATE building method. Construct a geodesic design mini-dome that can be used as a greenhouse, gazebo, garden shed, or any of a dozen other uses. Free

573 QUICK AND EASY HOUSE PAINTING

An informative new brochure from Wagner identifies common painting problems and the correct solutions. Topics discussed in the 16-page, illustrated brochure include surface preparation techniques, safety tips, paint selection advice and other hints. Free

574 WOOD APPRECIATION

Wood respects, wood reflects. Booklet describes tips on finishing and refinishing interior and exterior wood finishes. Color chart and name of nearest dealer are also provided. From Minwax. Free

575 IDEAS SET IN CONCRETE

Transform your yard into an outdoor entertainment area in just a few summer weekends. Sakrete Cement gives you step-by-step instruction for 18 different projects in this 16-page booklet. 75e

576 FEEL STRONGER, WORK LONGER

Dri-Dek® is an interlocking, vinyl floor tile that forms a light, portable surface. Soothes the aches, pains, and discomfort of standing. And, with six colors to choose from, adds good looks as well as safety to your shop. Sample and brochure from Kendall Plastics. Free

577 THE FIREPLACE THAT CAME IN FROM THE COLD

Majestic would like you to know that its Warm Majic¹⁹ fireplace has been rated 41-43% efficient by the Wood Heating Alliance. Two brochures offer full-color performance report and do-it-yourself installation planner, including chimney system. \$1.00

578 RESILIENT FLOORING

24-page illustrated booklet gives helpful advice on shopping for modern vinyl floors. Information on no-wax surfaces, cushioning, installation and upkeep. Plus, how to decorate and important questions to ask the salesperson. From Armstrong, Free

579 MAKING A REAL ENTRANCE

A Therma-Tru replacement door system can give your house a whole new look. In addition to acquiring an eye-catching "entrance" you can achieve energy savings and more security. 4-color, illustrated brochure. Free

580 THE RENOVATOR'S INNOVATIONS

48-page color catalog of hard-to-find hardware, plumbing and lighting products that will add a distinctive touch to your home. Most are made of solid brass, wrought iron, oak or porcelain. Many are authentic reproductions ... all newly made. From The Renovator's Supply. \$2.00

581 MAKING THE MOST OF SPACE

Compact range-sink-refrigerator combinations can solve your space problems. King Refrigerator offers an illustrated catalog describing its kitchen units. Free

582 HERE'S INEXPENSIVE LUXURY

Complete ThermaSol Health Spa fits into the same space as a standard size bathtub. Includes a combination bath and whirlpool tub, steam bath and shower enclosed with acrylic walls, ceilings and sliding glass doors. Illustrated brochure. \$2.00

583 SUPER POWER

The Fox Super Shop is a multi-purpose power tool. Seven basic wood and metal functions, including table saw, disc sander and wood lathe. Operator's manual illustrates each function and its many features. 50e

584 POWER WHEELS

Bike Motor and Go-Kart catalog offers discount prices on fuel efficient front and rear bike motors. Complete Go-Kart and mini-bikes and parts to build your own kart. From Northern Hydraulics. \$1,00

585 TOOL CHEST FOR A MASTER

Compact tool chest looks like a museum showpiece, but is durable enough for a professional. For handling ease, it's actually two boxes with well-organized hand tool compartments that allow easy access. Complete plans, including handy dolly, from POPULAR MECHANICS Project Library. \$4.75

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No. 1 album No. 1 song Center-fold, plus the hit title. Rage In



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Top 10 album, Top 10 Every Little Thing She Does Is Magic: smash Spirits In World:



'Album Of The Year"! Grammy-winner (Just Like)

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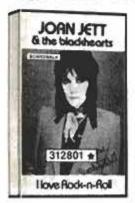
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PLUS THE "GOLD BOX" TV BON if you join the Club now and agree to buy 8 more selections (at regular Club prices) in the coming 3 years

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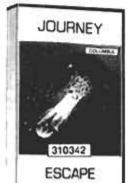
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Top 10 Title hit; Crimson And Clover: (I'm Gonna) Run Away: Little Drummer Boy



No. 1 album with smash Top 10 Juke Box Hero, Waiting for A Girl Like You: Urgent



Top 10 Open Arms. Who's Crying Now Don't Stop Believin' Stone In Love



313361 • CONWAY TWITTY
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313213 * EARL KLUGH GRAZY FOR YOU
313197 + DIRENA RASTON (IN COLORS) WINCOMS STAND
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Columbia Record & Tape Club, P.O. Box 1130 Terre Haute, Indiana 47811

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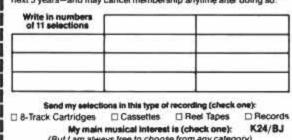
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I am enclosing check or money order for \$1.86 (that's 16 for my 11 introductory selections, plus \$1.85 for shipping and handling). Please accept my application under the terms outlined in this advertisement. I agree to buy 8 more tapes or records (at regular Club prices) in the next 3 years—and may cancel membership anytime after doing so.



(But I am always free to choose from any category) □ Easy Listening 2 □ Teen Hits 7 □ Classical 1 II.lazz 4 (nn reel tanes) Country 5 (no reel tapes)

Print First Name Last Name

City

State

Do You Have A Telephone: (Check one)

Yes

No This offer not evaluate in APO FPO Alexas Haway inverto Rico, write to details of alternative offer Canadian residents will be serviced from Toronto.

Also send my first selection for at teast a 50% discount, for which I am also enclosing additional payment of \$3.99 if then need buy only 7 more selections (at regular Club prices) in the next three years.

K26/BL

★ not available in reel tapes □ Available on records and cass ettes only

Yes, just take a look at the superstars and super hits that the Columbia Record & Tape Club now offers. To get any 11 of these records or tapes right away, simply fill in and mail the application together with your check or money order for \$1.86 as payment (that's 1¢ for your first 11 selections, plus \$1.85 to cover shipping and handling). In exchange, you agree to buy 8 more tapes or records (at regular Club prices) in the next three years-and you may cancel your membership at any time after doing so

How the Club operates: every four weeks (13 times a year) you'll receive the Club's music magazine, which describes the Selection of the Month for each musical interest...plus hundreds of alternates from every field of music. In addition, up to six times a year you may receive offers of Spe-cial Selections, usually at a discount off regular Club prices, for a total of up to 19 buying opportunities

If you wish to receive the Selection of the Month or the Special Selection, you need do nothing it will be shipped automatically. If you prefer an alternate selection, or



Note: All applications are subject to review; Columbia House reserves the right to reject any application

none at all, fill in the response card always provided and mail it by the date specified. You will always have at least 10 days to make your decision. If you ever receive any Selection without having had at least 10 days to decide, you may return it at our expense

The tapes and records you order during your membership will be billed at regular Club prices, which currently are \$7.98 to \$9.98 - plus shipping and handling. (Multi-ple-unit sets and Double Selections may be somewhat higher) And if you decide to continue as a member after completing your enrollment agreement, you'll be eligible for our money-saving bonus plan

10-Day Free Trial: we'll send details of the Club's operation with your introductory shipment. If you are not satisfied for any reason whatsoever, just return everything within 10 days for a full refund and you will have no further obligation. So act now!

Special Start-Your-Membership-Now Offer: you may also choose your first selection right now—and we'll give it to you for at least 50% off regular Club prices (only \$3.99). Enclose pay-ment now and you'll receive it with your 11 introductory selections. This half-price purchase reduces your membership obligation immediately—you'll then be required to buy just 7 more selections (instead of 8) in the next three years. Just check box in application and fill in number you want.

Here's the "Gold Box" you've seen on TV —fill it in and get an extra selection

K27/BQ K25/AF

NOW! Get in on the PROFITS SMALL ENGINE REPAIR

rk part time, full time y step of the way.

No doubt about it . . as a small engine pro, you can cash in on the huge demand for qualified men in one of America's fastest growing industries. You'll be able to command top hourly rates of from \$10.00 to \$15.00 per hour — and

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Massional Tools and Equipment PLUS 4 hp Engine YOURS TO KEEP All at No Extra Cost. NO EXPERIENCE NECESSARY

You don't need to be a 'born mechanic' or have any prior experience. Lessons are fully illustrated ... so clear you can't go wrong, and with our famous 'learn-by-doing' method you get practical 'hands-on' experience.

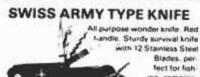






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PM LOOKS AT



he Echo PB-200 is a portable. handheld, gas-powered blower. Its compact size and light weight make it easy to handle. It also has an attachable shoulder strap.

The blower's wide range of airstream velocity, from a gentle breeze to a high-speed blast moving at about 103 mph, makes it effective in numerous outdoor cleanup jobs. You can use it yearround for sidewalk, patio and lawn sweeping, light snow removal from drives and walkways and for removing leaves and twigs from rain gutters. The airstream velocity is controlled by a trigger throttle in the handle. The on/off ignition switch is on top of the handle.

When I tried out the blower, the engine started easily. It uses 20 parts of regular-grade gas to one part motor oil. The tank holds 13.5 fluid ounces and fuel consumption is rated at 20.3 fluid ounces per hour.-Rosario Capotosto

SPECIFICATIONS-PORTABLE BLOWER

Model: No. PB-200 Dimensions (L×W×H): B.2 × 11.1 × 14.0 m. Dry weight: 9.7 lbs. Engine type: Air-cooled, two-stroke, single-cylinder Fuel: Premixed two-stroke fuel Max. air speed: 196.9 ft. per sec. Max. air volume: 280 cu. ft. per minute

Price: \$129.95 Manufacturer: Echo Inc., Box 578, Northbrook, III. 60062 Sweeping gutters is a simple job with the portable blower. Here, a section of pipe (Inset) is removed for easier handling.



A trigger throttle and a stop switch are on the handle. A loop to hook on a shoulder strap is at each side of the handle.



The fuel tank holds enough fuel to operate the engine for about 40 minutes.

Married Charles were list and sections of



What a pleasure it is to enjoy outdoor living all summer long without being tormented by pesky insects! Now you can really savor outdoor fun, swimming, gardening, cookouts and even naps outside — free from stings and bites! Yes, you can have the most wonderful summer of your life with this new ELECTRONIC BUG KILLER. It attracts insects having phototropic vision with an almost invisible Blue Light. Then, as they fly to these irresistable rays, they are killed instantly on contact!

100% SAFE TO USE!

This ELECTRONIC BUG KILLER is absolutely SAFE around children or pets because it uses no dangerous chemicals, poisons, fumes or insecticides. Insects are killed by an Electrified Grid that uses only 3 Volts of electricity! That's about as much as a small flashlite! Such a small shock that birds, pets, children and adults can scarcely feel it — yet it kills every insect on contact — INSTANTLY! Less than 3 pennies keep it going all day and night — indoors or outside! This new model is made of rugged, hi-impact material, styled like a charming antique Colonial Carriage Lamp. Just hang it on your patio, in the yard, around the pool or in your home to be free of annoying insects!

TRY IT ON THIS FREE HOME TRIAL!

We'll rush this amazing ELECTRONIC BUG KILLER to you on a 30 Day Home Trial. It comes ready to operate — just plug into any outlet and it's in action. See how safe it is! How it clears your garden or patio of flying insects. You must be 100% sataisfied! Otherwise, return within 30 Days for your money back (Less Postage & Handling).

No. 4855—ELECTRONIC BUG KILLER \$12.95 plus \$1.55 for Postage & Handling. SPECIAL: Two for \$24.00 plus \$2.00 Postage & Handling. Three for only \$35.00 plus \$2.25 Postage & Handling. N.Y.S. Residents must add sales for

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Please RUSH the ELECTRONIC be completely satisfied or 1 m back (less Postage & Handling).	BUG KILLERS as checked below, I must ay return within 30 Days for my money
 □ ONE 8ug Killer — \$12.95 + □ TWO Bug Killers — \$24.00 - □ THREE 8ug Killers — \$35.00 	\$1.55 Postage & Handling + \$2.00 Postage & Handling
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P. O. BOX A99 - HICKORY, N. C. 28601

Turning a scaled-down wood replica

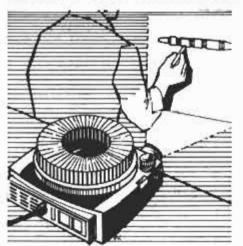
y wife wanted to restore an old (circa 1750) spinning wheel of the type having a long leg and two shorter legs. The shorter legs were missing and had to be reconstructed. My solution involved making a scaled-down pattern of the longer leg and using it as a guide to turn the shorter legs. You can apply the same technique to turn a scaleddown replica or to make an enlarged copy of any other item.

First, I had to calculate the required length of the missing shorter legs. Based on similar spinning wheels seen at museums and in books, I arrived at the approximate length needed (13 in.).

Next, I made a cardboard pattern for the missing legs in the following manner. I took a photograph of the larger, existing leg. Then I used an old magic lantern (an opaque projector will also do) to project the image onto white cardboard taped to the wall. When the projected image was the exact size needed for the smaller legs, my wife traced the shape and cut out the pattern.

You can use this technique to make a pattern by taking a slide of the original item and using a slide projector to enlarge the image on the cardboard.

Next, I used the cardboard pattern to turn the legs. Since a woodworking lathe wasn't available, I built the following setup on my metal lathe. First, I measured the exact center between the head- and tailstocks and made a mark there. Then I placed the workpiece in the lathe. After marking the middle of a 1×2 , I centered it across the lathe from



Shoot a slide of the leg to be duplicated, then project it the exact size needed onto white cardboard and trace the shape.

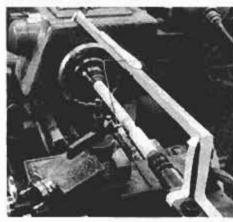


The two smaller spinning-wheel legs are scaled-down versions of the larger leg.

headstock to tailstock (see photo, bottom right). Shims were used to level the 1×2, and it was taped to prevent movement.

I thumbtacked the cardboard pattern on top of the 1×2, directly over the workpiece. I ground a suitable tool to turn the wood and tightened it in the tool holder. I also secured in the tool holder a length of 10-gauge copper wire ground to a point at its free end. The wire was easily bent into a Z-shape so its pointed end touched the cardboard pattern above the workpiece.

The rest of the job was a matter of setting the metal-cutting lathe at high speed and cranking the tool in and out along the length of the workpiece as the pointer followed the pattern. By making a scaleddown pattern of the larger, existing leg. I was able to reconstruct the two shorter legs of my spinning wheelwithout ever having seen the original legs.—Ivan E. Prall



Author, lacking a wood lathe, created the scaled legs using his metal lathe and the setup shown above. The cutting tool is locked in the tool holder; the template to be followed by wire is tacked to a 1x2.

POPULAR MECHANICS

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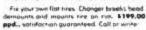
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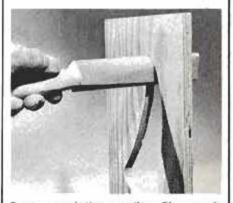
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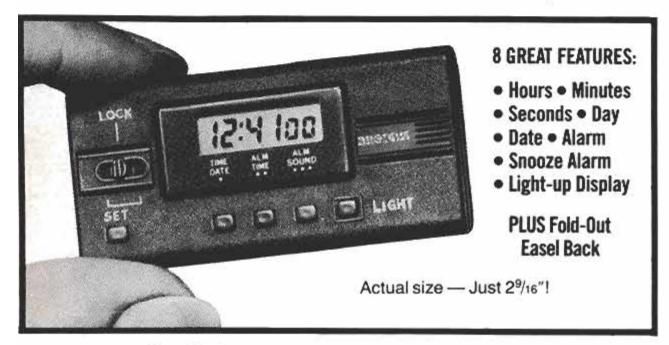
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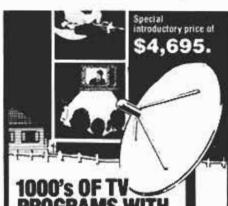
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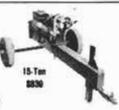
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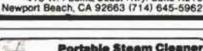
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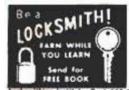
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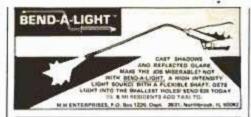
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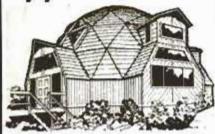
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If ever a publication was accurately named, it's Moneysworth. This offer is highly recommended.

Moneysworth mcmlxxxii. TM reg.

ATWO SEAT SPORTS CAR WITH A 7-FOOT TRUNK.

THE 1982 SR5 SPORT TRUCK. Twoseat sports cars are fine...if all you travel with is driving gloves and sunglasses.

But if you like to bring your toys when you go places, take this two-seat sports machine.

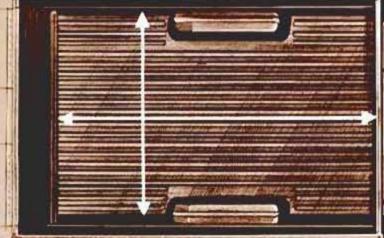
Its 7-foot trunk holds a dirt bike, a snowmobile...almost any payload up to 1100 pounds.

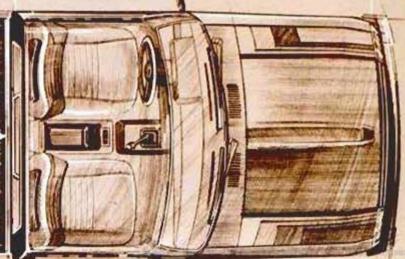
And inside, the Toyota SR5 Long Bed Sport Truck really is outfitted like a sports car. With bucket seats. Big, blacked-out gauges. Even an AM/FM/MPX stereo radio. All standard. Start the 2.4 liter 4-cylinder overhead cam engine and the sports car feeling gets even better. You've got power-assisted disc brakes up front. A sporty 5-speed overdrive transmission. Or you can order Toyota's innovative 4-speed automatic overdrive.

TOYOTA

And one more thing—mileage you wouldn't expect from a sports car! The Toyota SR5 is rated at 34 EPA Estimated Highway MPG, 24 EPA Estimated MPG. Remember: Compare this estimate to the EPA "Estimated MPG" of other small trucks. You may get different mileage, depending on how fast you drive, weather conditions, and trip length. Actual highway mileage will probably be less than the EPA "Highway Estimate."

The Toyota SR5 Sport Truck. It isn't hard to believe you really are in a sports car. Just don't look back!





Which is the lowest menthol 100's? (Hint: it's not Carlton.)

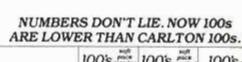
NOW is the lowest menthol 100s. But it's easy to see why some people aren't sure. They've had a lot of confusing numbers thrown at them for quite a while.

Just look at the chart at the right and see for uourself.

The truth is that NOW 100s Soft Pack Menthol at 2 mg tar contains less than half the tar of Carlton 100s Soft Pack Menthol at 5 mg. And NOW Menthol 100s is by far and away lower in tar than <u>any</u> other Menthol 100s whatsoever.

Which is the lowest 100s menthol?

No need to guess-NOW.



 NOW
 2mg
 2mg
 100's mesther
 100's hor

 NOW
 2mg
 2mg
 0.01mg

 CARLTON
 5mg
 5mg
 1mg

All far numbers are ay per organite by FTC method.



NOW

The lowest

The lowest in tar of all brands.

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Warning: The Surgeon General Has Determined That Cigarette Smoking Is Dangerous to Your Health. BOX, BOX 100's: Less than 0.01 mg, "tar", 0.001 mg, nicotine, SOFT PACK 85's FILTER, MENTHOL: 1 mg, "tar", 0.1 mg, nicotine, SOFT PACK 100's FILTER, MENTHOL: 2 mg, "tar", 0.2 mg, nicotine, av, per cigarette by FTC method.

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